

CRAWLEY BOROUGH COUNCIL
PLANNING COMMITTEE - 7 June 2016
REPORT NO: PES/204

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<u>ITEM</u>	<u>REF NO</u>	<u>LOCATION</u>	<u>RECOMMEND</u>
001	CR/2015/0740/ARM	PHASE 2A FORGE WOOD (NORTH EAST SECTOR) CRAWLEY	APPROVE
002	CR/2016/0048/ARM	PHASE 1, FORGE WOOD, (NORTH EAST SECTOR), CRAWLEY	APPROVE
003	CR/2016/0109/FUL	5 BARNWOOD, POUND HILL, CRAWLEY, RH10 7TH	REFUSE
004	CR/2016/0115/FUL	23 ASH ROAD, THREE BRIDGES, CRAWLEY, RH10 1SG	PERMIT
005	CR/2016/0166/FUL	56 - 60 NORTH ROAD, THREE BRIDGES, CRAWLEY	PERMIT
006	CR/2016/0193/FUL	40 CRABBET ROAD, THREE BRIDGES, CRAWLEY, RH10 1NE	PERMIT
007	CR/2016/0216/FUL	UNIT 1, POWER AVENUE, (PART OF FORMER GSK SITE), NORTHGATE, CRAWLEY	PERMIT
008	CR/2016/0225/FUL	27 FORGE ROAD, THREE BRIDGES, CRAWLEY	PERMIT
009	CR/2016/0262/FUL	ROUNDBOUT ADJ TO MAIDENBOWER DRIVE, MAIDENBOWER, CRAWLEY	PERMIT
010	CR/2016/0349/FUL	UNIT D, WOOLBOROUGH LANE, NORTHGATE, CRAWLEY	PERMIT

REFERENCE NO: CR/2015/0740/ARM

LOCATION: [PHASE 2A FORGE WOOD \(NORTH EAST SECTOR\) CRAWLEY](#)
PROPOSAL: APPROVAL OF RESERVED MATTERS FOR PHASE 2A FOR 90 DWELLINGS, A PUMPING STATION AND RELATED WORKS PURSUANT TO CR/1998/0039/OUT FOR ERECTION OF UP TO 1900 DWELLINGS, 5000 SQ.M OF USE CLASS B1, B2 & B8 EMPLOYMENT FLOORSPACE, 2500 SQ.M OF RETAIL FLOORSPACE, A LOCAL CENTRE/COMMUNITY CENTRE (INCLUDING A COMMUNITY HALL), A NEW PRIMARY SCHOOL, RECREATIONAL OPEN SPACE, LANDSCAPING, THE RELOCATION OF THE 132KV OHV POWER LINE ADJACENT TO THE M23, INFRASTRUCTURE AND MEANS OF ACCESS

TARGET DECISION DATE: 29 January 2016

CASE OFFICER: Mrs V. Cheesman

APPLICANTS NAME: Persimmon Homes & Taylor Wimpey

AGENTS NAME: Pegasus Planning Group Limited

PLANS & DRAWINGS CONSIDERED:

P.1137-06 Site Location Plan, P.1137-03 Site Survey - Topographical Survey, P.1137-05-1 Newton Elevations and Floor Plans, P.1137-05-2 Corfe Elevations and Floor Plans, P.1137-05-3 Chedworth Elevations and Floor Plans, P.1137-05-4 Leicester Elevations and Floor Plans, P.1036-05-5 Clayton CA1 Elevations and Floor Plans, P.1036-05-6 Clayton CA3 Elevations and Floor Plans, P.1137-05-7 Hatfield Elevations and Floor Plans, P.1137-05-8 Hatfield & Clayton Elevations and Floor Plans, P.1036-05-9 Hanbury Elevations and Floor Plans, P.1036-05-10 WP2420A Elevations and Floor Plans (Affordable), P.1036-05-12 WP3502C Elevations and Floor Plans (Affordable), P.1036-05-13 Garages Elevations and Floor Plans, P.1137-05-11 Phase 2A Plot by Plot Schedule, P.1137-13 Enclosure Details, TPP7827-2A-02 Tree Protection Plan, P.1137-08 Rev B Phase 2A Materials Plan, P.1137 15A - Affordable Housing Location Plan, P.1137 05-15A - Apartment Block A Floor Plans, P.1137 05-14A - Apartment Block A Elevations, T258 26A - Pumping station, P 1137-12A Refuse Strategy Plan, P 1137 -04B Building Heights, P1137 05-11A - WP3520A Elevations and Floorplans, P1137-20 Bin Store Details, P1137 -02B - Garden Area Assessment Plan, P1137-01E Phase 2a- Site Layout, P1137 05-16B - Apartment Block B Elevations, P1137 05-17B Apartment Block B Floor plans, P775/20 Rev C - Finished Levels - Sheet 1 of 2, P775/21 Rev B - Finished Levels Plan Sheet 2 of 2, P775/3 Rev A - Swept Path - Large Refuse Vehicle, P775/4 Rev B - Vehicle swept path - large car, P775/22 - Private Construction Details, P258/23 - Adoptable Construction Details, 667-193B - Phase 2a 1 of 3 - Soft Landscaping, 667-194B - Phase 2a 2 of 3 Soft Landscaping, 667-195D - Phase 2a 3 of 3 Soft Landscaping, 667-196B - Phase 2a 1 of 3 Hard Landscaping, 667-197B - Phase 2a Hard Landscaping 2 of 3, 667-198C - Phase 2a - Hard Landscaping 3 of 3, P1137 15B - Affordable Housing Location Plan, P.1137 18A 01 Juliette Balcony, P.1137 18A 02 Brick Corbelling, P.1137 18A 03 Brick Coursing, P.1137 18A 04 Canopies, P.1137 18A 05 Chimneys , P.1137 18A 06 Creasing Tile Feature, P.1137 18A 07 Decorative barge board, P.1137 18A 08 Stone Coursing, P.1137 18A 09 Tile hanging, P.1137 18A 10 Window Cills, P.1137 18A 11 Window Headers, P.1137 18A 12 Window Styles 1 of 2, P.1137 18A 13 Window Styles 2 of 2

ADDENDUM REPORT

REASON FOR REPORTING TO COMMITTEE:-

- 1.1 Members will recall that at the April 11th meeting of the Committee this application was considered and it was resolved to approve the proposal subject to the following:

*The issue of the S73 permission (CR/2015/0552/NCC) and following this;
Amendment of the application description and further publicity and,
To impose any additional condition (s) in respect of the design details for the dwellings.*

- 1.2 The s73 application to vary the conditions attached to the original outline permission remains undetermined as further discussions are ongoing with the applicants as to the appropriate mechanism for tying in the reserved matters approvals to the new decision notice for the S73. In addition there are continuing discussions regarding noise issues for phases 2B and 2C. It now appears likely that this permission would not be ready to be issued for some months.
- 1.3 This delay means that the development of Phase 2A cannot proceed in the meantime and could result in the slower delivery of both market and affordable housing. The applicants have therefore asked for Phase 2A to be revisited to assess whether this phase can be determined pursuant to the original outline as this would allow for an earlier approval and start on site.
- 1.4 Having looked in detail at the application, and the reasons put forward by the applicants, Officers are of the view that the development, as proposed, would be in substantial accordance with the Masterplan dated May 2006 CSA/667/020 Rev F and the design statement (July 2006 updated May 2009) as well as the subsequently approved Infrastructure Reserved Matters (CR/2015/0628/ARM).

RESPONSES RECEIVED:-

- 2.1 None.

PLANNING CONSIDERATIONS:-

- 3.1 This application shows housing in an area that was identified for residential development in the original Masterplan and has not been enlarged from that approved therein. Whilst the internal road layout now proposed has been amended since the original Masterplan, this was included in the Phase 2 Infrastructure and has been approved under CR/2015/0628/ARM above.
- 3.2 Further trees will be removed in Phase 2A than was approved in the original Masterplan. This removal is required as a result of the amended road layout which has already been approved in the Infrastructure Reserved Matters and as a result of adjustments to the housing layout. The Arboriculture Officer raises no objection and replacement planting will be provided as part of the overall landscaping scheme.
- 3.3 Further details of the design and elevational treatment have now been received and are considered acceptable. The plans and drawings have been updated accordingly and no additional conditions are required.

CONCLUSIONS:-

- 4.1 It is considered that Phase 2A as proposed is in substantial accordance with the approved Masterplan and design statement and can now be approved without detrimental impact on other sub phases within Phase 2. The applicants will be required to comply with the conditions attached to the original outline permission and any subsequent amendment as well as the details conditions relevant to this sub phase.

RECOMMENDATION RE: CR/2015/0740/ARM

Approve subject to the conditions recommended in the earlier report to April Committee (attached) as updated.

REPORT CONSIDERED AT 11 APRIL 2016 COMMITTEE

CRAWLEY BOROUGH COUNCIL
DEVELOPMENT CONTROL COMMITTEE - 11 April 2016
REPORT NO: PES/183

ITEM NO: 003

REFERENCE NO: CR/2015/0740/ARM

LOCATION: PHASE 2A FORGE WOOD (NORTH EAST SECTOR) CRAWLEY
PROPOSAL: APPROVAL OF RESERVED MATTERS FOR PHASE 2A FOR 90 DWELLINGS, A PUMPING STATION AND RELATED WORKS PURSUANT TO CR/1998/0039/OUT FOR ERECTION OF UP TO 1900 DWELLINGS, 5000 SQ.M OF USE CLASS B1, B2 & B8 EMPLOYMENT FLOORSPACE, 2500 SQ.M OF RETAIL FLOORSPACE, A LOCAL CENTRE/COMMUNITY CENTRE (INCLUDING A COMMUNITY HALL), A NEW PRIMARY SCHOOL, RECREATIONAL OPEN SPACE, LANDSCAPING, THE RELOCATION OF THE 132KV OHV POWER LINE ADJACENT TO THE M23, INFRASTRUCTURE AND MEANS OF ACCESS

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CONSULTEE NOTIFICATIONS & RESPONSES:-

1. GAL - Planning Department No objection but GAL have noted the requirement for the dwellings to be suitably mitigated against aircraft noise (condition 34).
2. GAL - Aerodrome Safeguarding No objection subject to condition and informative

3.	Network Rail	No objection (representation Network Rail refer to relates to Phase 2b only). Informatives recommended to ensure the development does not impact on the safe operation of the railway.
4.	Environment Agency	Unable to agree drainage strategy due to lack of information, additional supporting information required on drainage calculations, catchments for 2a and long term maintenance. – (note amended document now submitted – see drainage section in report)
5.	WSSC - Highways	No objections in principle – comments on parking layout and level of visitor parking
6.	Thames Water	Requesting Grampian style condition is imposed as consider there is inadequate sewerage capacity to accommodate the development, informative recommended.
7.	Sussex Building Control Partnership	No comments received
8.	Police	Design advice provided 2 particular points of detail on the layout raised concerning the lack of natural surveillance for the parking bays at adjacent to plot 86 and alternative boundary solution for plot 75 suggested.
9.	CBC - Drainage Officer	No objection to amended Surface Water Strategy dated December 2015
10.	CBC - Property Division	No objection
11.	CBC - Housing Enabling & Development Manager	Objection – 40% affordable not provided, concern about location of dwellings next to railway and number of 3 bed 4 person units which do not meet housing need. (note all phase 2 reserved matters now submitted which show 40% affordable, unit mix under negotiation for phases 2c and 2d)
12.	CBC - Planning Arboricultural Officer	No objection provided works are in accordance with the recommendations in the arboricultural impact assessment.
13.	UK Power Networks	No objection.
14.	CBC - Environment Team	No comments
15.	CBC - Environmental Health	No comments received.
16.	Cycle Forum	No comments received
17.	CBC - Refuse & Recycling Team	Objection to positioning of communal collection points which appear to have an excessive carry distance for large bins, car parks and drives need to accommodate weight and ready access of refuse vehicle.
18.	Southern Water	No comments received.
19.	CBC - FP - Urban Design	Detailed design advice provided.
20.	Ecology Officer - Mike Bird	No objection.
21.	WSSC - Surface Water Drainage (SWD)	No objection provided detailed drainage designs provided to include provision for long term maintenance as part of any SuDs scheme.
22.	National Air Traffic Services (NATS)	No objection

NEIGHBOUR NOTIFICATIONS:-

This application was publicised via press and site notices.

RESPONSES RECEIVED:-

None.

REASON FOR REPORTING TO COMMITTEE:-

The application is part of Forge Wood in which CBC has a land interest, there are also objections to the proposal contrary to the Officer recommendation.

THE APPLICATION SITE:-

- 1.1 The application site relates to a 2.95 ha land parcel in the north-west corner of the Forge Wood. The site is former agricultural land used for grazing with tree belts intersecting the land marking the extent of former field boundaries. The site is bounded by the railway line to the west, agricultural land to the north, the planned central parkland for Forge Wood to the east and will adjoin further residential development comprising phase 2c to the south (currently agricultural land).
- 1.2 The site is relatively level and on its eastern side is partially within the Gatwick Stream Floodplain. There are 2 ditches intersecting the site one running west/east across the northern part of the site and a second which runs close to the eastern boundary, both join in the north eastern corner of the phase and appear to drain into Gatwick Stream to the east. The land is identified as an archaeologically sensitive area and there are a group of trees located in the western portion of the site identified to be retained. A public footpath from Tinsley Green to Steers Lane crosses through the north east corner of the site.

THE PROPOSED DEVELOPMENT:-

- 2.1 The application seeks approval of reserved matters for the erection of 90 dwellings and a pumping station. The dwellings comprise a mixture of houses (1 x 5 bedroom, 5 x 4 bedroom, 22 x 3 bedroom and 26 x 2 bedroom units) and flats (18 x 2 bedroom 3 person and 18 x 2 bedroom 4 person). The flats which are to be provided as 6 x 3 storey blocks are located on the eastern side of the phase and would face over the central parkland. The dwellings are all sited to the north of the central spine road serving phase 2 and are configured around 2 cul-de-sacs and one loop road all accessed off the main spine road. A pumping station is proposed in the far north of the phase to the north of the flats (plots 1-6), the location of this infrastructure was approved as part of Phase 1a however, the facility has been subject to minor changes and these have now been included in the details of this phase.
- 2.2 In terms of scale the development comprises primarily 2 storey houses and 3 storey flats and the designs proposed relate to 3 of the 5 character areas agreed in the Design Statement with flats and houses on the eastern side following the architecture of the 'parkland edge', those facing the central spine road designed as 'Main Street' and those to the west of the spine road designed as 'neighbourhood housing'.
- 2.3 24% of the housing within Phase 2a is proposed to be affordable, these units are to be located adjacent to the pumping station and on the western portion of the site. The applicants state the dwellings in this phase would be at a density of 31 dwellings per hectare.
- 2.4 The phase is accessed via the new spine road which connects phase 2 to the wider neighbourhood via 2 bridge crossings. The principal point of access for this phase would be via the northern crossing directly to the east of this phase (2a) providing a vehicular link to phase 1, the neighbourhood centre and north onto Steers Lane. The second access is via phases 2c and 2b to the south of phase 2a and across a southern bridge which connects to Phase 1a and the A2011.
- 2.5 The following documents have been provided in support of the application.
 - Design Statement
 - Planning Statement
 - Phase 2 Sustainable Surface Water Drainage Strategy (Amended document December 2015)
 - Phase 2a Residential Landscape Management Plan
 - Arboricultural Impact Assessment
 - Ecological Assessment

PLANNING HISTORY:-

- 3.1 The planning history for this site which includes reference to this phase, has been set out in detail in sections 1 and 2 of the Officer report CR/2015/0522/NCC (Item 1 on this agenda). Forge Wood was originally granted outline planning permission under application reference CR/1998/0039/OUT.
- 3.2 In respect of this sub-phase, in January 2016 approval was granted under CR/2016/0628/ARM for the design of the main internal distributor road and a number of smaller secondary roads serving Phase 2 of the development. The fixing of the vertical and horizontal alignment of the distributor road has allowed the 'reserved matter' applications for the various parts of the Phase 2 to be brought forward as land parcels.
- 3.3 As set out in paragraph 3.1 above, the outline planning permission (CR/1998/0039/OUT) as recommended to be varied under application CR/2015/0522/NCC is directly relevant to the determination of this reserved matter application and of particular relevance are the following planning conditions as worded from that expected decision:
- 3.3.1 Condition 1 requires the development to be in substantial accordance with the Masterplan drawing and Design Statement provided to support the outline application. It also requires that the average density of the development shall be 41 dwellings per hectare in the residential areas.
- 3.3.2 Condition 2 requires a detailed design and access statement to be submitted with each phase or sub phase of the development and for this document to demonstrate how the phase meets the objectives of the Design Statement.
- 3.3.3 Condition 3 seeks delivery of the development in accordance with the approved Phasing Plan P.1036_13D-2.
- 3.3.4 Condition 5 requires approval of the siting, design, external appearance and landscaping be obtained before work is commenced on that phase.
- 3.3.5 Condition 8 sets out in detail the particulars required to accompany each reserved matter application while condition 9 specifies the particulars required for the site tree protection measures.
- 3.4 Specific conditions are also imposed which influence design features for the development these include:
- 3.4.1 Condition 40 which states that the number of parking spaces for the development shall not exceed the standards set on in Annex B of the Council's "Planning Obligations and S106 Agreements" SPD. It should be noted that this condition is proposed to be deleted under the NCC application as this condition is not considered necessary as the adequacy of the parking provision is assessed as part of the layout against the adopted parking standards as part of the Reserved Matters application. Condition 41 requires the parking provision once laid out to be only be used for this purpose.
- 3.4.2 Condition 65 which removes permitted development rights for ground floor side / rear extensions and roof alterations for all the dwellings.
- 3.5 With regard to affordable housing condition 62 requires no less than 40% of the total number of dwellings within each phase to be affordable housing while condition 63 requires agreement of a 'scheme' for the provision of affordable housing in each phase of the development to be submitted to and approved by the Local Planning Authority (LPA).

PLANNING POLICY:-

National Planning Policy Framework

- 4.1 This document is a material consideration. It emphasises that the purpose of the planning system is to contribute to the achievement of sustainable development and that planning decisions must be taken in accordance with the development plan unless material considerations indicate otherwise. Listed within the core principles in this document is the requirement that the planning system should *"always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings (paragraph 17)"*. Paragraphs 56 to 66 expand upon this principle stating amongst other things that development plans should have robust and comprehensive policies which set out the quality of development expected for an area.

- 4.2 Section 38 (6) of the Planning and Compulsory Purchase Act 2004 and section 70(2) of the Town and Country Planning Act 1990 states that applications for planning permission must be determined in accordance with the relevant provisions of the Development Plan unless material considerations indicate otherwise.

Crawley Borough Local Plan 2015-2030 (adopted December 2015)

- 4.3 Housing policy H2 identifies Forge Wood as a key deliverable housing site for up to 1,900 dwellings with the period to 2020. Policy H3 requires all housing development to provide a mix of dwelling types and sizes to address the nature of local housing needs and market demand. Affordable and low cost housing should be provided as part of the housing mix and policy H4 specifies the requirement for affordable housing. In this case the affordable housing requirements for Forge Wood are already addressed through conditions 1, 62 and 63 on the outline permission / NCC application.
- 4.4 Overarching policy SD1 sets out the presumption in favour of sustainable development in line with 6 strategic objectives which include progress towards climate change commitments, providing a safe and secure environment for residents and visitors and meeting the social and economic needs of the current and future population and policy CH1 supports development in line with the neighbourhood principle (of which this sub-phase of Forge Wood complies with).
- 4.5 Policy CH2 sets out the principles for good urban design and states:
To assist in the creation, retention or enhancement of successful places in Crawley, development proposals will be required to:
- a) respond to and reinforce locally distinctive patterns of development and landscape character and protect and/or enhance heritage assets;*
 - b) create continuous frontages onto streets and spaces enclosed by development which clearly defines private and public areas;*
 - c) create public spaces and routes that are attractive, safe, uncluttered and which work effectively for all in society, including disabled and elderly people;*
 - d) make places that connect with each other and are easy to move through, putting people before traffic and integrating land uses and transport networks;*
 - e) provide recognisable routes, intersections and landmarks to help people find their way around;*
 - f) consider flexible development forms that can respond to changing social, technological and economic conditions; and*
 - g) provide diversity and choice through a mix of compatible developments and uses that work together to create viable places that respond to local needs.*
- Applications must include information that demonstrates that these principles would be achieved, or not compromised, through the proposed development.*
- 4.6 Policy CH3 sets out the normal requirements of all development and requires proposals to be based on a thorough understanding of the significance and distinctiveness of the site and its immediate and wider context, be of high quality in terms of landscape and architectural design and relate sympathetically to their surroundings in terms of scale, density, height, massing, orientation, views, landscape, layout, details and materials. In addition, proposals must provide a good standard of amenity for future residents in compliance with internal space standards and not cause unreasonable harm to the amenity of the surrounding area by way of overlooking, dominance or overshadowing, traffic generation and general activity. The policy requires the retention of existing individual or groups of trees that contribute positively to the area and seeks to ensure sufficient space for trees to reach maturity particularly when located in private gardens to ensure dwellings receive adequate daylight. Development should also meet its operational needs in respect of parking, access, refuse storage etc. The policy also requires the development to incorporate 'Secure by Design principles' to reduce crime, consider community safety measures and demonstrate design quality through 'Building for life' criteria.
- 4.7 Policy CH4 requires the comprehensive and efficient use of land to ensure the proper phasing of development over a wider area.

- 4.8 Policy CH5 sets out the internal space standards for all new dwellings in line with the Nationally Described Space Standards and also requires development to include appropriate provision of external space (currently set out in SPG4).
- 4.9 Policy CH6 seeks to ensure landscape proposals for residential development contribute to the character and appearance of the town and seek to ensure 1 new tree for each dwelling and where trees are lost seek mitigation in line with the published replacement standards.
- 4.10 Policy CH7 identifies the central parkland area directly to the east of the application site as structural landscaping and an important feature that should be protected and enhanced, this link has been considered and identified as integral landscape feature in the overall master planning of Forge Wood. In respect of access, policy CH11 requires proposals that detract on the character of a right of way or other type of recreational route to be adequately mitigated.
- 4.11 Policy CH12 seeks to protect heritage assets. This site has been identified as an archaeologically sensitive area and condition 19 on the outline /NCC application has covered these requirements.
- 4.12 Policy EC4 requires that where development is proposed adjacent to main employment areas, the economic function of the employment area is not restrained.
- 4.13 Policy ENV2 requires all proposals to encourage biodiversity where appropriate and to refuse proposals where there would be significant harm to protected habitats or species unless harm can be appropriately mitigated.
- 4.14 Policy ENV6 sets out the requirement for all development to maximise carbon efficiency with all new dwellings being required to have strengthened on-site energy performance standards while policy EN9 seeks to ensure development is planned and designed to minimise the impact on water resources and promote water efficiency.
- 4.15 Policy ENV8 seeks to ensure development proposals must avoid areas which are exposed to an unacceptable risk from flooding and must not increase the risk of flooding elsewhere (the suitability of this site was considered as part of the outline application, through the overall masterplan layout and via planning conditions).
- 4.16 Policy ENV11 seeks to protect people's quality of life from unacceptable noise impacts, these issues were addressed via conditions on the outline / NCC application.
- 4.17 Policy IN2 requires residential development to be designed and connected to high quality communications infrastructure to ensure that fibre optic or other cabling does not need to be retrofitted.
- 4.18 Policy IN4 requires development to meet its needs when assessed against the Council's car parking and cycle parking standards.

Supplementary Planning Guidance and Documents

- 4.19 The following documents are also relevant in the consideration of this application:
- Planning Obligations and Section 106 Agreements Supplementary Planning Document' – This document sets out the LPA parking and cycle parking standards for new development.
 - Supplementary Planning Guidance Note 4 – 'Standards for Private Outdoor Space' – provides guidance on the minimum standard of provision for private outdoor space.

PLANNING CONSIDERATIONS:-

- 5.1 The principle of the new neighbourhood with its overall masterplan, the amount of development and the access arrangements for the site at the principal road junctions are established by the outline planning permission and subsequent variation of condition application if permitted (CR/2015/0522/NCC). The NCC application if permitted would vary certain conditions on the outline

approval and would in effect result in the issue of a new outline planning application for Forge Wood. This proposal is a reserved matter application and the LPA cannot refuse to approve any 'Reserved Matter' on grounds which go to the principle of the development but must consider the details relevant only to that particular application. The purpose of a Reserved Matter application is to agree the detail for the development and in respect of this proposal comprises the detailed design and layout of the phase 2a residential dwellings.

5.2 The key considerations in relation to this application are:

- Is the development in substantial accordance with the Masterplan and design statement?
- Design approach and the links to the rest of Phase 2 and wider neighbourhood.
- Housing mix and affordable housing.
- Adequacy of the dwellings for future occupiers including parking and servicing requirements.
- Noise impacts and sustainable design considerations.
- Impacts on ecology, trees and landscaping.
- Drainage.
- Foul drainage.

Is the development in substantial accordance with the Masterplan and design statement?

5.3 Phase 2a is in substantial accordance with the Masterplan and design statement as amended under application CR/2015/0522/NCC, the plan development is therefore considered to be in accordance with condition 1.

5.4 In respect of density condition 1 requires each phase to be an average of 41 dph in residential areas. Phase 2a has been calculated at 31 dph, phase 2b has a density of 54 dph and while 2c and 2d are yet to be fully considered it is likely that the average 41 dph density for the phase2 is likely to be achieved. The wider phase 2 layout therefore compensates for the lower density in 2a and on this basis is considered to accord with condition 1.

5.5 The design statement was submitted with the outline planning permission and revised by the NCC application and requires the development to be in substantial accordance with this document. The Phase 2a layout is in substantial accordance with this document.

Design approach and the links to the rest of Phase 2 and wider neighbourhood

5.6 As set out in section 2.0 above the proposed layout of 2 storey houses and 3 storey flats continues the design approach established in phase 1 of Forge Wood. The house types and designs are similar to those that were approved in the Phase 1 character areas and the proposed detailing and palette of materials are also broadly the same and in line with the approved Design Statement.

5.7 In terms of design detailing, the elevations of the flats mirror the designs approved in phase 1c 'Parkland Edge' and while some of the design features and details have been supplied, a number of detailed features appear to be missing from the submission and further details to ensure a complete submission have been requested from the applicants. Similarly, additional design details for example the brick detailing, tile hanging and cill detailing have been provided in this submission although again not all the decorative details appear to be provided for the house types and clarification has been sought from the applicants. It is considered that any omissions to the detailing could be controlled via a condition and Members will be advised if any additional condition is necessary at the meeting.

5.8 As part of the consultation process a number of detailed comments were received concerning individual plot details and designs some of which have been addressed by the applicant, it is considered that in general the level of detail on the plots is adequate. The flats have been redesigned to address initial concerns relating to overlooking and to maximise the outlook these units enjoy to the central parkland.

5.9 The layout is considered to create a lively active street frontage and legible housing layout. Amended plans have been received which improve the relationship of plots 80-90 in relation to the railway,

enlarge the garden sizes and address the police concerns in relation to natural surveillance, this layout is now considered acceptable.

- 5.10 Overall the layout is considered to integrate well with other key linkages identified in the master plan and includes an improved footpath / cycle link in the northern part of the site to Radford Road, the approved spine road through phase 2 and allows legible and overlooked footpath links to the parkland further east and to other areas of open space in the layout. The overall layout is considered to allow for comprehensive development and should not prejudice the development potential of land parcel 2c to the south.

Housing mix and affordable housing

- 5.11 Within Phase 2a 24% of the housing is proposed to be affordable. It should be noted that condition 62 requires that not less than 40% affordable housing provision in each phase of the development and the applicants have demonstrated that the overall amount will be met in Phase 2 as a whole, the breakdown being as follows:
- Phase 2a – 90 dwellings – 24% affordable
 - Phase 2b – 159 dwellings – 40% affordable
 - Phase 2c – 251 dwellings – 45% affordable
 - Phase 2d – 75 dwellings - ,40% affordable
- While Phase 2a is short of affordable units it has been demonstrated that 40% can be provided within the phase overall and on this basis, the layout is considered acceptable.
- 5.12 In terms of the units provided in Phase 2a the layout provides for 6 x 2 bedroom flats , 2 x 2 bedroom houses and 1 x 3 bedroom house for affordable shared ownership and, 6 x 2 bedroom houses and 7 x 3 bedroom houses for affordable rent. This is considered a suitable tenure and dwelling mix.
- 5.13 The affordable houses are located in a single cluster in the northwest of the site, while ideally these would have been better interspersed in the layout, the number of properties (16 dwellings) is not considered excessive. The proposed house designs do differ from the private market housing, the most obvious difference being the lack of garages in this part of the layout however, while the designs do differ the level and form of detailing between the house types is consistent and it is considered that overall the design proposed designs would result in a tenure blind layout. In respect of the affordable flats which are located on the north east corner of the site, the design of these units are identical to the private flats on the opposite side of the road.

Adequacy of the dwellings for future occupiers including parking and servicing requirements

- 5.14 All the affordable units comply with the adopted internal space standards set out in policy CH5. In respect of private amenity space the dwellings have gardens and amended plans have been provided to bring the majority of affordable units in line with garden space standards. 3 units have under sized gardens however, the space provided is usable shape and layout and these units have an open outlook onto farmland to the rear and a tree belt to the front, in this case this slight under provision is considered acceptable. While the design of the flats do not incorporate any balconies, it is considered that given the location of the blocks immediately adjacent to the central parkland that the future occupants have easy access to a substantial area of amenity space and therefore provision is adequate in the site layout.
- 5.15 2 parking spaces have been provided for each affordable dwelling and each affordable flat has one allocated parking space. Additional visitor parking is provided in the layout for both the flats and dwellings and it is considered that this level of provision accords with the adopted parking standards. Refuse and cycle storage is also provided for the units in line with standards.
- 5.16 In respect of the private dwellings, all the units comply with the adopted space standards set out in CH5 for internal space and all meet the garden space standards.
- 5.17 The majority of the dwellings have a garage and 2 tandem parking spaces. There is one 2 bed semi-detached house type that does not have a garage some units have two on plot parking spaces while

14 properties have on-street parking for the dwellings to the front of the property at a ratio of 1.5 spaces per dwelling which is considered adequate and accords with adopted parking standards.

- 5.18 The private flats are also considered to meet adopted space standards and have parking provision at 1 space per dwelling plus a limited number of visitor spaces. WSCC have commented on the limited number of visitor spaces within the layout and in particular in the parking courts for the private flats. Amended plans have been received which have increased the number of visitor parking spaces in some parts of the layout. There are a further 7 visitor spaces along the main spine road which could be accessed by visitors to the flats and overall, in the absence of any specific requirement in the adopted parking standards to provide visitor spaces, the level of provision within this layout is considered reasonable.
- 5.19 Bin and cycle stores are to be provided for each block of flats and locations are indicated on the layout however, the precise details of the cycle stores and timing of this and the refuse provision are to be controlled via condition. In terms of servicing the refuse areas, additional information has been provided to demonstrate that refuse vehicles can manoeuvre into the parking courts to service the bin stores and therefore reduce carry distances for operatives, the issue of surfacing is a management issue however the drawings assume large vehicles accessing the parking courts and this has been factored into the surfacing design.
- 5.20 In terms of the overall layout, in general the separation distances between windows are considered adequate between the houses. Within the flat layout there is one aspect of the design where the window separation distances are inadequate to ensure privacy between occupants of the flats (this is the facing elevations between the flats at the main access plots 13-36 and the facing windows in the far northern elevation of plots 13-36 facing onto plots 7-12 and at the southern elevation where the flats face flats proposed in phase 2c. Amended plans have been submitted re-configuring the flats to ensure only secondary windows are positioned on this elevations. The master bedrooms for these units are now dual aspect however, it is considered appropriate to impose a condition to ensure the facing windows remain obscure glazed to safeguard privacy and ensure layout of phase 2c is not compromised.

Noise impacts and sustainable design considerations

- 5.21 Due to the site's proximity to Gatwick, the impact of aircraft noise from both a single and two runway airport on the development was a key consideration at the Planning Inquiries and a number of conditions were imposed by the Inspector to safeguard the amenities of future occupiers, GAL have also commented on this application about the noise impact on the new dwellings. The applicants have submitted information under discharge of condition request CR/1998/0039/CC14 to seek to address the requirements of condition 34 which requires a 'scheme' to demonstrate the dwellings have sufficient protection against noise. This information has been the subject of extensive discussions between the LPA and the applicants and the LPA have provided a technical specification of works which if implemented, will address the requirements of the condition. At the time of preparing this report the applicants are still disputing these requirements. The technical specification demonstrates that an appropriate scheme can be implemented without significant changes to the external appearance of the dwellings which are to be considered as part of this 'reserved matters' application and the acoustic requirements for Phase 2 will be dealt with through the discharge of condition 34.
- 5.22 Condition 34 also requires a 'scheme' to demonstrate the dwellings have sufficient protection from other noise sources including the railway line and other mixed noise sources (such as the employment uses to the west of the railway). No specific details have been proposed by the applicants to address this condition in respect of phase 2a and it is expected that the scheme will be required to mitigate noise and vibration from these sources, it would be down to the applicants to ensure this requirement is discharged via condition.
- 5.23 In respect of condition 23 as proposed by the NCC, this requires an assessment of the buildings to meet sustainable design and construction measures offsetting 10% of predicted carbon emissions (energy) through a combination of sustainable design and construction or on-site renewable energy.

The sustainable construction or 'fabric first' approach would have no impact on the exterior of the buildings as designed however, the introduction of renewables most likely PV panels would have a visual impact on the design of the dwellings and it is proposed that in such circumstances details of any PV panels to be installed is controlled via a condition to safeguard the visual quality of the development.

Impacts on ecology, trees and landscaping

- 5.24 An ecological report was provided with the application, the report identifies the land which is primarily grazing land interspersed with semi-mature trees. It is not ecologically rich however there has been one grass snake reported on the eastern edge of the site suggesting potential habitat for reptiles. Natural England's standing advice for reptiles has been considered in relation to grass snakes, the loss of this habitat was accepted with the grant of outline permission. The proposed mitigation is to ensure grass snakes are not harmed and translocation measures are recommended for any specimens found to suitable habitats on the wider site. Provided the measures proposed in the ecological report are implemented, the Council's ecological advisor raises no objection.
- 5.25 It is accepted that as a greenfield site and through the outline approval of the masterplan layout that there would be a considerable loss of countryside and trees. Within phase 2a two substantial groups of trees are to be retained comprising a group of hawthorn, alder, oak and ash in the northern part of the site and a row of oak trees in the south west corner, these were identified as important tree groups on the masterplan. The arboricultural officer has raised no objection to the layout on the basis that the trees to be retained are protected in accordance with the submitted tree protection measures.
- 5.26 In this case it not considered that the policy requirements of CH6 are appropriate in respect to the expected level of tree replacement given the nature of the development in this case. As part of the layout the applicants are proposing to mitigate some tree loss as part of the landscaping along streets and in gardens and the landscape design is considered appropriate within the new residential layout.

Drainage

- 5.27 There is a need to ensure that there is an effective drainage strategy for the construction phase of this development and to serve the Forge Wood as a whole. The drainage strategy submitted for the site pursuant to condition 16 (but not yet fully discharged) assumes that sewer alignments would be under the main roads and this is shown in the submitted plans approved for the phase 2 spine roads under CR/2015/0628/ARM along with the agreed positioning of the swales as indicated on the site wide strategy.
- 5.28 The phase 2a application was submitted in parallel with phase 2b and the spine road applications with an identical drainage strategy supplied for each application. In order to determine the spine road application, the drainage strategy was subject to further discussions following the initial concerns raised by the Environment Agency and Drainage Officer. An amended document was prepared and the overall drainage strategy was agreed for phase 2 as part of the spine road application. This agreed document has now been submitted for phase 2a. Phase 2a drains into Swale 1 directly to the east of the site, the surface water from this Phase will be directed towards the Gatwick Stream.
- 5.29 The outstanding matter regarding drainage is the future long term maintenance and management of the drainage features and this is the subject of ongoing discussions in respect of the site wide drainage strategy under condition 16. The intended approach is that the drainage would be the responsibility of a private management company and finalised details of the arrangements and financial safeguards are still to be submitted and agreed for this condition.

Foul drainage

- 5.30 Thames Water have commented that the existing waste water infrastructure is unable to accommodate the needs of the development and have recommended that a 'Grampian Style' condition be imposed requiring an on and off-site drainage strategy for the proposed works. It is not appropriate to impose a condition relating to strategic infrastructure to a reserved matter application

as such issues should have been conditioned at outline stage. The appeal Inspector did not impose a specific condition in respect of foul water drainage.

- 5.31 The applicants have confirmed that they have a foul drainage strategy and agreement with Thames Water which covers the whole development which comprises 2 connections one each side of the Balcombe Road to serve the neighbourhood.

CONCLUSIONS:-

- 6.1 This application for Phase 2a residential in Forge Wood is considered to have addressed the design parameters and relevant conditions imposed on the outline planning permission (as proposed to be amended by CR/2015/0552/NCC) and is considered to be in substantial accordance with the Masterplan and Design Statement.
- 6.2 The design and layout is considered to follow the approach established with the 'reserved matters' in Phase 1 and would therefore allow this phase to continue in a similar style and character to create a cohesive development pattern and distinctive character to Forge Wood. The design details and dwelling styles are generally replicated established from Phase 1 and are considered appropriate.
- 6.3 All the dwellings are considered to comply with the adopted internal space standards, have generally adequate gardens and sufficient parking provision. The layout in terms of its design, landscaping and indicative palette of materials provides an attractive and legible layout and safeguards the privacy and amenity of future occupants.
- 6.4 The layout is considered to appropriately address the environmental safeguards set out in the outline conditions in terms of ecology, noise and drainage subject in some cases to further work to discharge the relevant conditions. The application has been considered in relation to the wider requirements on the outline permission and notwithstanding some of the conditions and details are yet to be fully discharged, the approval of the design and layout of these dwellings now would not compromise the requirements of these conditions and their eventual discharge subject to the appropriate level of detail.
- 6.5 It is therefore recommended to APPROVE reserved matters for phase 2a as set out below:

RECOMMENDATION RE: CR/2015/0740/ARM

APPROVE : subject to the following conditions

- 1 The development hereby permitted shall not be carried out other than in accordance with the approved plans as listed in this Decision Notice save as varied by the conditions hereafter.
REASON: For the avoidance of doubt and in the interests of proper planning.
- 2 Provision shall be made for high speed broadband to serve all dwellings on the development in accordance with details to be submitted to and agreed in writing by the Local Planning Authority.
REASON: To ensure delivery of high quality communications in accordance with Policy IN2 of the Crawley Borough Local Plan 2015-2030.
- 3 Provision shall be made for combined aerial facilities to serve all flat development in accordance with details to be submitted to and agreed in writing by the Local Planning Authority.
REASON: In the interests of the visual amenities in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.
- 4 Prior to first occupation, the windows on side elevation of apartment block B plots 13 to 36 (serving the en-suite and bedroom 1 of each apartment) shall be glazed with obscure glass in accordance with details to be submitted to and agreed in writing by the Local Planning Authority. Those windows shall be permanently maintained in accordance with the agreed details at all times thereafter.
REASON: To protect the amenities and privacy of the future occupiers in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

- 5 Before the properties approved in this phase are occupied, details of the design and location of lighting to be provided in the forecourt and parking areas shall be submitted to and approved in writing by the Local Planning Authority, and thereafter implemented in accordance with the agreed details.
REASON: To safeguard the appearance of the development and the amenities of the area in accordance with Policies GD2 and CH3 of the Crawley Borough Local Plan 2015 -2030.
- 6 Details of the secure cycle store(s) for the flats shall be submitted to and approved in writing by the Local Planning Authority such drawings to show the siting and design thereof. Once agreed in writing the cycle stores shall be constructed and operational prior to the first occupation of the flats for which they are intended to serve.
REASON: To safeguard the appearance of the development and ensure the operational requirements of the development are met in accordance with Policy CH3 of the Crawley Borough Local Plan 2015 - 2030.
- 7 The refuse stores shown on approved drawing P1137-20 shall be constructed and operational prior to the first occupation of the flats for which they are intended to serve.
REASON: To safeguard the appearance of the development and ensure the operational requirements of the development are met in accordance with Policy CH3 of the Crawley Borough Local Plan 2015 - 2030.
- 8 No solar panels or renewables which may be required pursuant to the discharge of condition 23 shall be installed unless and until full details of their design and location are submitted to and agreed by the Local Planning Authority in writing. The works shall thereafter be implemented in accordance with the agreed details.
REASON: To control the design of the development in detail in accordance with GD3 of the Crawley Borough Local Plan 2015 – 2030.
- 9 No development, including site works of any description shall take place on the site unless and until all the trees/bushes/hedges to be retained on the site have been protected in accordance with the tree protection plan 7827/2A/02 and in accordance with measures in the submitted Arboricultural method statement 7827/Phase2A/so. Within the areas thereby fenced off the existing ground level shall be neither raised nor lowered and no materials, temporary buildings, plant machinery or surplus soil shall be placed or stored thereon without the prior written approval of the Local Planning Authority. If any trenches for services are required in the fenced off areas they shall be excavated and backfilled by hand and any roots with a diameter of 25mm or more shall be left un-severed.
REASON: To ensure the retention and maintenance of trees and vegetation which is an important feature of the area in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.
- 10 The construction of the development shall be undertaken in accordance with the safeguards detailed within the submitted Ecological Assessment - Phase 2a Residential (October 2015) provided by Ecology Solutions.
REASON: To ensure that the ecology of the site is protected and enhanced in accordance with policy ENV2 of the Crawley Borough Local Plan 2015-2030 and Para 118 of the NPPF 2012.
- 11 All landscaping works shall be carried out in accordance with the approved soft landscaping specification scheme (drawing numbers CSA/667/193B, CSA/667/164B and CSA/667/195C). No alterations to the approved landscaping scheme are to take place unless submitted to and approved in writing by the Local Planning Authority.
REASON: In the interests of visual amenity and because the scheme has been designed to mitigate bird hazard and avoid endangering the safe movements off aircraft and the operation of Gatwick Airport through the attraction of birds.
- 12 All planting, seeding or turfing comprised in the approved details of landscaping within Phase 2a shall be carried out in the first planting and seeding seasons following the occupation of the buildings or the completion of the development, whichever is the sooner, and any trees or plants which within a period of five years from the completion of the development die, are removed, or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species, unless the Local Planning Authority gives written consent to any variation.

REASON: In the interests of amenity and of the environment of the development in the accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

- 13 The dwellings hereby approved shall not be occupied until the SUDS drainage measures for the catchment area in which phase 2a is situated have been fully implemented in accordance with the details agreed under CR/2015/0628/ARM.

REASON: To ensure that the development is satisfactorily drained in accordance with policy ENV8 of the Crawley Borough Local Plan 2015-2030.

INFORMATIVES

- 1 Given the nature of the proposed development it is possible that a crane may be required during its construction. We would, therefore, draw the applicant's attention to the requirement within the British Standard Code of Practice for the safe use of Cranes, for crane operators to consult the aerodrome before erecting a crane in close proximity to an aerodrome. Gatwick Airport requires a minimum of four weeks notice. For crane queries/applications please email gal.safeguarding@gatwickairport.com The crane process is explained further in Advice Note 4, 'Cranes and Other Construction Issues', (available from www.aoa.org.uk/policy-campaigns/operations-safety/)
- 2 The Gatwick Stream is a designated main river. The prior written consent of the Environment Agency is required under Section 109 Water Resources Act 1991 for any works in, over or under the change of the watercourse or on the banks within eight metres of the top off the bank. Any new outfall structure to the Gatwick Stream would require the Environment Agencies prior Consent.
- 3 Network Rail strongly recommends the developer contacts AssetProtectionsSussex@networkrail.co.uk prior to any works commencing on site, and also to agree an Asset Protection Agreement with us to enable approval of detailed works. More information can also be obtained from our website at www.networkrail.co.uk/asp/1538.aspx
- 4 The applicant's attention is drawn to the comments and advice provided by Network rail in its response to this application. A copy of the advisory note provided by Network Rail is attached to this decision notice.
- 5 A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. The development should demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquires should be direct to the Risk Management Team <tel:02035779483> or email wwriskmanagement@thameswater.co.uk Application forms should be completed online via www.thameswater.co.uk/wastewaterquality

NPPF Statement

1. In determining this planning application, the Local Planning Authority assessed the proposal against all material considerations and has worked with the applicant in a positive and proactive manner based on seeking solutions where possible and required, by:
 - Providing advice through pre-application discussions/correspondence.
 - Liaising with consultees/respondents/applicant/agent and discussing the proposal where considered appropriate during the course of the determination of the application.
 - Seeking amended plans/additional information to address identified issues during the course of the application.

This decision has been taken in accordance with the requirement in the National Planning Policy Framework, as set out in article 35, of the Town and Country Planning (Development Management Procedure) Order 2015.



Crawley Borough Council,
Town Hall,
The Boulevard,
Crawley,
West Sussex RH10 1UZ

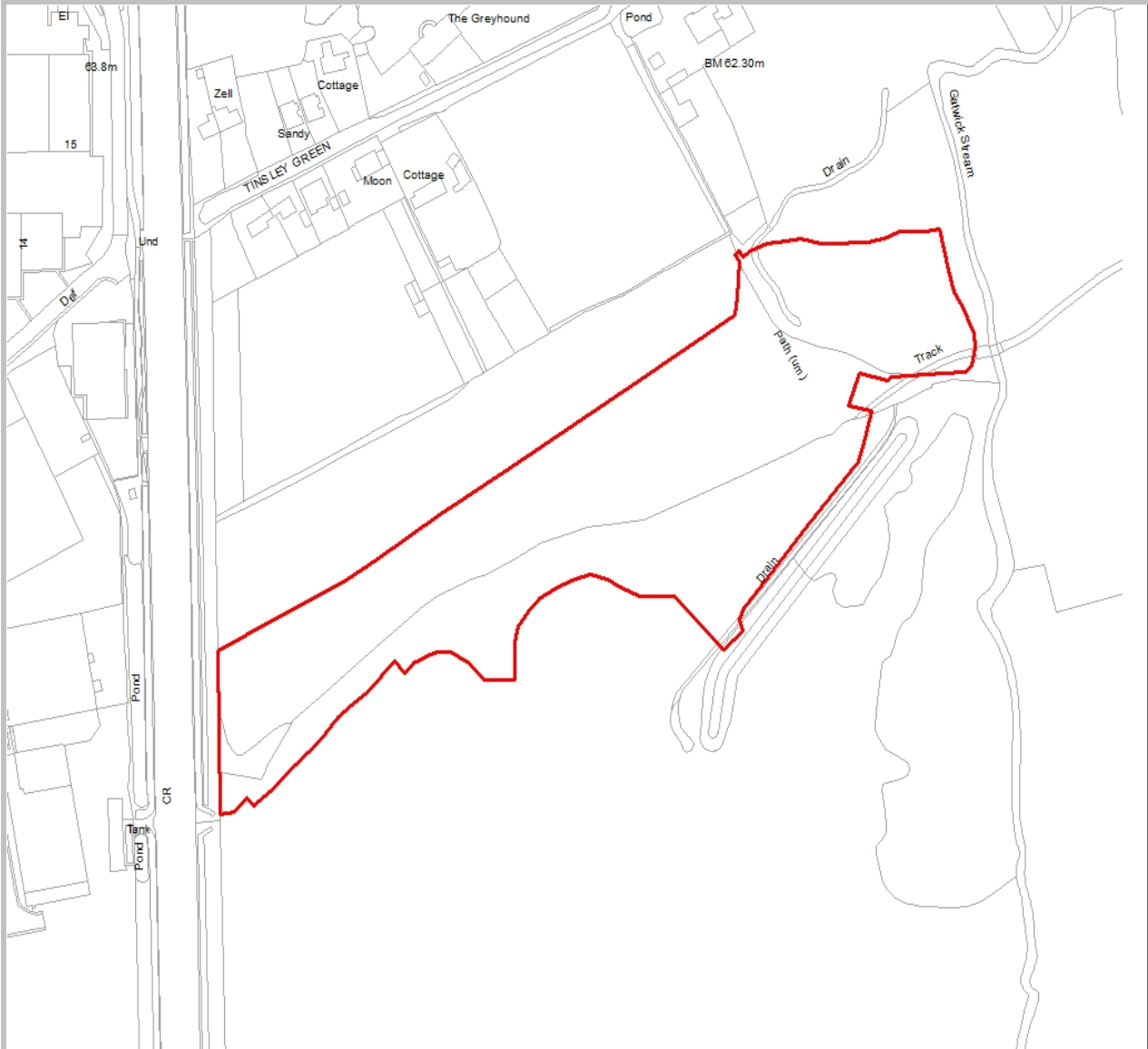
Tel: 01293 438000
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CR/2015/0740/ARM

Date 20 May 2016

Approx. Scale 1:1,250

**PHASE 2A FORGE WOOD (NORTH EAST
SECTOR) CRAWLEY**



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REFERENCE NO: CR/2016/0048/ARM

LOCATION: [PHASE 1, FORGE WOOD, \(NORTH EAST SECTOR\), CRAWLEY](#)

PROPOSAL: APPROVAL OF RESERVED MATTERS FOR PHASE 1 FOR THE ERECTION OF A PRIMARY SCHOOL WITH SPORTS PITCHES AND COURTS, PLAYING FIELDS, PLAYGROUND, CAR AND CYCLE PARKING, INTERNAL ACCESS ROADS, FOOTPATHS AND CIRCULATION AREAS, HARD AND SOFT LANDSCAPING, AND OTHER ASSOCIATED INFRASTRUCTURE AND ENGINEERING WORKS (AMENDED PLANS RECEIVED)

TARGET DECISION DATE: 13 May 2016

CASE OFFICER: Mrs V. Cheesman

APPLICANTS NAME: Persimmon Homes & Taylor Wimpey

AGENTS NAME: Pegasus Group

PLANS & DRAWINGS CONSIDERED:

31895_AP(00)03_C_Ground Floor Plan, 31895_AP(00)04_B_Roof Plan, C15 1604-SK-54 P2 Foul Drainage-SK-54, 9126-01_Tree Constraints Plan, C15 1604-C-10-P2 Existing Levels, C15 1604-SK-55-P4 Swept Path Analysis, C15 1604-SK-56-P3 Swept Path Analysis, 31895_AP(00)01_A_Site Location Plan, 31895_AP(00)02_C_Proposed Site Plan, 31895_AP(00)05_F_Elevations, 31895_AP(00)06_F_Perspectives, 31895_LL(90)03_B Cycle and Scooter Provision Details, L(SK)01-B Landscape Site Plan , L(SK)02-C-Planting Plan & Schedule, L(SK)03-B Hardscape Plan, L(SK)04-Rev B Tree Protection Plan, L(SK)05-B Site Areas, 31895_LL(90)02_A Boundary treatment Plan, C15 1604-SK-57 Swept Path Analysis, 31895_AS(SK)12- B Site Access and Security, C15 1604-C-01-P3 Drainage Strategy, C15 1604-C-11-P4 Proposed Levels

NOTIFICATIONS & RESPONSES:-

1. CBC - Property Division – no objection
2. CBC - Planning Arboricultural Officer - no objection
3. CBC - Drainage Officer – the drainage submission is well written, comprehensive and follows good principles. However, full micro drainage calculations, details of the drainage arrangements for the sports pitches and management /maintenance of the proposals will be required. Drainage condition required.
4. CBC - Refuse & Recycling Team - comment on swept path analysis for collection vehicles
5. CBC - FP - Energy Efficiency & Sustainability –. The revised BREEAM pre-assessment report identifies the credits targeted by the proposal and if achieved will meet the 'Very Good' rating. The wording of the condition needs further consideration in terms of the timing of the submission of the post-construction completion certificate.
6. CBC - FP - Urban Design - Comments on layout and design detailing. The majority have been addressed in revised layouts and details

7. CBC - Environmental Health - further noise data and information regarding the acoustic properties of the building and noise mitigation measures are required
8. GAL - Planning Department - further details are required regarding the specific noise mitigation measures for protection against noise of the internal teaching areas and the external teaching environment.
9. GAL - Aerodrome Safeguarding - no objection subject to a condition requiring a bird hazard management plan and a cranes informative
10. National Air Traffic Services (NATS) - no safeguarding objection
11. Network Rail - no objections
12. WSCC - Highways - no objection subject to conditions
13. Environment Agency - no comments received
14. Thames Water - Requesting Grampian style condition is imposed as Thames Water consider there is inadequate sewerage capacity.
15. Southern Water – The water supply services at this location is the responsibility of Independent Water Networks Ltd. There is an inset agreement in place between Southern Water and Independent Water Networks Ltd for the supply of water services. The connection point to the water supply network and the agreed demand flow rate must be complied with.
16. Independent Water Networks Ltd – no objection
17. Sussex Building Control Partnership - no comments received
18. Police - boundary treatments are acceptable, controlled parking for staff and visitors is welcomed. An access system into the school should be installed. Appropriate measures to safely cross the road to the school should be considered.
19. UK Power Networks - no objections
20. Cycle Forum and CTC Right to Ride - adequate cycle and scooter parking facilities should be provided
21. Ecology Advisor – no objection subject to the implementation of the mitigation and enhancement recommendations set out in the ecological assessment report.
22. WSCC - Surface Water Drainage (SWD) - no comments received

NEIGHBOUR NOTIFICATIONS:-

The application was publicised via press and site notices.

RESPONSES RECEIVED:-

3 letters received from residents of 2 households in Somerley Drive – objections on the grounds of additional traffic and parking, detrimental to highway safety, that the car park entrance and exit is to be opposite their properties and this differs from original master plan, loss of privacy, loss of view, and additional noise from school and use of sports facilities.

REASON FOR REPORTING TO COMMITTEE:-

The application is part of Forge Wood in which CBC has a land interest and is major development.

BACKGROUND:-

- 1.1 Outline planning permission (reference CR/1998/0039/OUT) was granted by the Secretary of State on the 16th February 2011, and established the principle of a mixed use neighbourhood to include up to 1,900 dwellings, 5,000 sq m of employment floorspace, 2,500 sq m of net retail space, a local centre/community centre, a primary school, recreational open space, landscaping, the relocation of a 132KV power line adjacent to the M23 and other associated works. The outline application included details of the access arrangements for the site including the design of the principal road junctions with all other matters of detail the subject of later applications for Approval of Reserved Matters in relation to siting, design and external appearance along with the associated landscaping.
- 1.2 More recently, application CR/2015/0552/NCC was considered by the Committee on 11th April 2016 regarding revisions to the outline permission. This proposes revisions to the Master Plan and variation of various conditions and associated plans. The Committee resolved to grant planning permission subject to satisfactory comments from WSCC highways, the drafting of condition 31 to relate to the fire service infrastructure, the re-drafting of condition 6 relating to the time periods for submission of reserved matters and production of an up-to-date list of plans. At the time of preparing this report these matters are ongoing and the decision has yet to be issued.
- 1.3 This proposal relates to land in Phase 1 of the neighbourhood. In respect of Phase 1, the reserved matters application has been approved for the spine road (CR/2012/0357/ARM), and there are 4 separate sub-phases, namely:
 - Phase 1A for 204 dwellings – CR/2013/0610/ARM - approved
 - Phase 1B for the local centre – CR/2014/0061/ARM - approved
 - Phase 1C for 50 flats – CR/2014/0062/ARM – approved
 - Phase 1D for a health centre – CR/2014/0063/ARM – to be determined.
 - Temporary Primary School – CR/2016/0048/ARM – approved.

THE APPLICATION SITE:-

- 2.1 The application site is 1.93 ha in area, and is located centrally within the northern part of Forge Wood. It forms part of phase 1 and is within the neighbourhood centre/School character area that is identified in the Design Statement. The site area corresponds to the location of the primary school as depicted on the Master Plan.
- 2.2 The site is positioned to the south of the proposed local centre (shops with flats above) and the community hall (Phase 1B – CR/2014/0061/ARM) with Somerley Drive along its eastern and southern boundaries, which is the main link road through the development. The flats at Hammingden Court and the site for the health centre (Phase 1D – CR/2014/0063/ARM – to be determined) lie to the north west, together with a semi-circular area of mature trees, which are to be retained.

THE PROPOSED DEVELOPMENT:-

- 3.1 The application seeks the approval of reserved matters for a 2 form entry (2FE) primary school for up to 420 pupils, together with playing fields, playgrounds, sports pitches and courts, car and cycle parking, footpaths and circulation areas, landscaping and associated infrastructure and engineering works.
- 3.2 The school building is positioned centrally on the application site, with the main pedestrian access point for pupils and drop off area being from the north west to encourage linked trips to the Local Centre and the multi use of its car park. This entrance is also immediately adjacent to the cycleway which runs north/south through this part of Forge Wood. The vehicular access point is on the other side of the site, to the east and is for staff, deliveries and visitors, with car parking facilities.

- 3.3 The site accommodates the required 2FE school, with land directly to the south (0.87ha) being reserved for possible future expansion of the school to 3FE if required for an additional 210 pupils. The design of the school has also been developed to achieve a layout that can be extended to accommodate the additional floorspace if required.
- 3.4 The school is some 2289sqm in floor area laid out in a single storey arrangement. The internal layout incorporates 2 wings on the western part of the building, for the key stage 1 and 2 pupils, each with pairs of classrooms for each year group, on either side of a central corridor/shared teaching space. There is also a central learning resources area with shared teaching facilities. This shared area allows for direct access to an indoor/outdoor classroom area, which incorporates a polycarbonate canopy. There is a second outdoor area with a canopy adjacent to the reception classrooms. The sports/dining hall is in the eastern part of the building and is laid out as a third wing. It is adjacent to the core facilities, staff and administrative areas. The main staff and visitor entrance to the school with office and reception area is located in this part of the building. A fourth wing is shown indicatively on the plans to the south of the staff rooms, and is the location for the additional floorspace to provide the 3FE accommodation, if so required and would complete the 'butterfly' footprint.
- 3.5 The proposed materials are brick and render panels with colour coated standing seam roofing. The roof has been designed as a 'kick up' roof with clerestory windows to provide light into the central corridor space and the main hall. The roofscape facing west is in one continuous form following the curvature of the building and a similar design is used for the section over the main hall facing east. There are a number of roof ventilators (or chimneys) on the roof.
- 3.6 Play areas and sports pitches are shown to the south and west of the school building, comprising hard and soft informal play areas and a hard surfaced PE area and soft PE areas with 3 football pitches. The play area for reception year pupils is situated in the northern part of the site, between the classrooms and the hall and is access directly from the classrooms and is to be artificial play turf with mounding.
- 3.7 The landscaping proposals also show a shelter with seats, a meadow and wildflower lawn and vegetable garden/allotment area, a nature garden, bee hives and bulb planted areas.
- 3.8 In terms of boundary treatment, the entire school grounds will be fenced with 1.8m high fencing, comprising hoop top railings to the public facing frontages and weld mesh fencing for the rest of the site. The fencing along the school car park street entrance will be 1.2m high hoop top fencing.
- 3.9 During the course of the consideration of this application amended plans and details have been submitted to address concerns raised, including design and layout issues, parking and sustainable construction.

PLANNING POLICY:-

The National Planning Policy Framework 2012 (NPPF)

- 4.1 The NPPF has a golden thread running through it which seeks to ensure a presumption in favour of sustainable development and states that there are three dimensions to sustainable development and the planning system performs an economic, social and environmental role. These roles are mutually dependent. The Framework requires applications to be determined in accordance with the development plan.
- 4.2 Relevant sections are:
- paragraph 14: presumption in favour of sustainable development – this means that development that accords with the development plan should be approved without delay, or where the development plan is absent, silent or relevant policies are out of date, unless there would be significant adverse impacts which would outweigh the benefits or it would be contrary to the policies in the NPPF
 - paragraph 17 : core planning principles

- section 7: requiring good design - this emphasises the importance of good design to achieve high quality and inclusive design for all development, including individual buildings, public and private spaces and wider development schemes. It is proper to seek to promote or reinforce local distinctiveness and the policies and decisions should address the integration of new development into the natural, built and historic environment.

Crawley 2030: The Crawley Borough Local Plan 2015-2030

- 4.3 The plan was adopted on 16th December 2015. Relevant policies include:
- 4.4 Policy SD1: 'Presumption in favour of Sustainable Development' states that the Council will take a positive approach to approving development which is sustainable.
- 4.5 Policy CH1 'Neighbourhood Principle' states the neighbourhood principle will be protected and enhanced by maintaining the neighbourhood structure of the town with a clear pattern of land uses and arrangement of open spaces and landscape features. Development within the neighbourhoods should mainly be residential, in-keeping with the existing neighbourhood.
- 4.6 Policy CH2 'Principles of Good Urban Design' seeks to assist in the creation, retention or enhancement of successful places in Crawley, with new development proposals required to: respond to and reinforce locally distinctive patterns of development and landscape character and protect and/or enhance heritage assets.
- 4.7 Policy CH3 'Normal Requirements of All New Development' states all proposals for development in Crawley will be required to make a positive contribution to the area; be of a high quality design, provide and retain a good standard of amenity for all nearby and future occupants of land and buildings and be able to meet its own operational requirements necessary for the safe and proper use of the site.
- 4.8 Policy ENV6 'Sustainable Design and Construction' states all new dwellings will be required to meet the strengthened on-site energy performance standards of Building Regulations, and any subsequent increased requirements.
- 4.9 Policy ENV8 'Development and Flood Risk' states that development proposals must avoid areas which are exposed to an unacceptable risk from flooding and must not increase the risk of flooding elsewhere. Flood risk assessments should be submitted where required and SUDS should be used to reduce run-off.
- 4.10 Policy ENV11 'Development and Noise' seeks to protect people's quality of life from unacceptable noise impacts and requires noise impact assessments where relevant.
- 4.11 Policy IN3 'Development and Requirements for Sustainable Transport' states that development should be concentrated in locations where sustainable travel patterns can be achieved. In addition, developments should meet the access needs they generate and not cause an unacceptable impact in terms of increased congestion or highway safety. Where appropriate, Transport Statements or Transport Assessments will be required.
- 4.12 Policy IN4 'Car and Cycle Parking Standards' states development will be permitted where the proposals provide the appropriate amount of car and cycle parking to meet its needs when it is assessed against the borough council's car and cycle parking standards.
- 4.13 Policy IN5 'Location and Provision of New Infrastructure' states that the provision of new or improved infrastructure in appropriate locations will be supported where the facilities are required to support the development or they add to the range and quality of facilities in the town. Major facilities should be located in the most sustainable locations and local community facilities should be located close to neighbourhood centres.

Supplementary Planning Guidance and Documents

- 4.14 Planning Obligations and S106 Agreements -- Supplementary Planning Document 2008: Annex B contains the adopted parking standards and infrastructure contributions.

PLANNING CONSIDERATIONS:-

5.1 The key issues in considering this application are considered to be:

- The principle of the development and its location within Forge Wood
- Impact on the visual amenities and character of the area
- Highway safety, access and parking
- The operational needs of the development
- Noise
- Surface Water Drainage
- Foul Drainage
- Sustainability measures
- Ecology, landscaping and trees
- Impact on nearby residents

The Principle of the Development and its location within Forge Wood

- 5.2 The outline planning permission (and the s73) includes provision for a primary school as a key component of the new neighbourhood within Phase 1 and a site is shown on the Master Plan and the Design Statement. The education requirements for Forge Wood are set out in the Education Position Statement (2009) which provided the agreement between the applicants and WSCC with regard to the school places required to support this development of up to 1900 dwellings. The mechanism to secure this provision is covered by condition 32 of the outline permission.
- 5.3 The Education Position Statement (2009) sets out that there would be a need for a 2FE school for up to 420 pupils to be provided within the development. A core site of 1.93ha in a central location is identified on the Master Plan, with an additional 0.87 of land to allow for expansion to a 3FE school if there is further housing in the locality (as per the Forge wood neighbourhood allocation of up to 2700 dwellings). It is also set out that temporary provision of up to 60 places may be required, depending on the timing of the 2FE provision.
- 5.4 Thus the principle of a school, in this location has been established at the outline stage and accords with the Education Position Statement.
- 5.5 In terms of its wider location, the proposed site is centrally positioned in the neighbourhood, being adjacent to the Local Centre and associated facilities. The Master Plan approved under CR/1998/0039/OUT depicts a school in this location, within Phase 1 and to the immediate south of the local centre. Thus this reserved matters application is consistent with the approved Master Plan.
- 5.6 The highway (Somerley Drive) to the north and east boundaries of the site is already in place, as is the pedestrian/cycle route to the north west. The site is well located in relation to the residential development, the current element of which is under construction in phases 1A and 1C, and so the majority of pupils will be able to walk to the school. In addition, in relation to Steers Lane, the site is well placed with regard to the main site access and the temporary bus service that currently runs to and from Three Bridges Station and Crawley Bus Station.

Impact on the visual amenities and character of the area

- 5.7 A contemporary design has been chosen for the school building, which Officers consider is an acceptable approach. The building is single storey with a kick up roof and utilises panels of render and brick with a colour coated standing seam roof. Samples of the materials have been provided. Following discussions at pre-application stage and throughout the course of this application, revised details have been submitted in relation to materials, windows and other features of the building and the landscaping to follow good urban design principles.

- 5.8 Whilst the applicants have not been able to agree to all requests for amendments, for example the continuous canopies and additional windows, overall the revisions are considered to be acceptable and address the concerns raised.

Highway safety, access and parking

- 5.9 Pedestrian access to the site for pupils will be from the north via an open plaza to the north of the main entrance gates, which would be reached by the footpath/cycle way that will run north/south through the wider development. The gates lead to a hard surfaced area encircling the school building, where parents would wait to collect their children and would give direct access to the individual classrooms. Scooter and cycle parking for pupils is to be provided. This area for waiting and collection has been enlarged on the amended plans and the cycle /scooter facilities have been increased. The revised plans that have been submitted are now considered to provide satisfactory arrangements for pupils and parents.
- 5.10 For pupils being transported to school by car, vehicular drop off/pick up will occur within the public car park, to the north of this school site, which serves the local centre. The car park is directly connected to the footpath /cycleway, which crosses Somerley Drive and leads to the plaza.
- 5.11 In terms of vehicular access into the application site itself, 2 access points are proposed to the east of the site, off Somerley Drive to serve the car park. The northernmost access point will be in/out and will lead to the visitor car park with 6 spaces, and an area for deliveries and emergency vehicles with a turning area. Leading south, across the front of the school, is the staff car park of 27 spaces, including 2 spaces to accessible standards. Access to this area is by a control boom and will operate on a one-way basis with egress to the south, back onto Somerley Drive.
- 5.12 A pedestrian access to the front entrance of the school will lead from Somerley Drive, constructed of block paving. Cycle storage facilities are to be provided adjacent to the entrance point.
- 5.13 WSCC highways have no highway objections but comment that yellow keep clear markings and flashing school advisory signs on the main spine road outside the school will be required for safety reasons. This would need to be the subject of a condition, together with conditions relating to the provision of the accesses and a school travel plan.

The operational needs of the development

- 5.14 The characteristic of a good development is that it can satisfactorily incorporate and accommodate the operational needs of that particular user. In particular, schools have specific access and parking requirements for pupils, parents, staff and deliveries, as assessed above. Furthermore, schools need to provide suitable outdoor space and provide for play and sports activities.
- 5.15 The proposals include 2 canopied areas to meet the requirements for outdoor learning and are positioned between the wings of the building so that they provide in effect central 'courtyards'. In addition, in the wider grounds of the school it is proposed to provide soft and hard landscaped areas for both formal and informal play.
- 5.16 To the west and south of the school building there are hard and soft informal play areas and a hard surfaced PE area. Further to the south there are proposed soft PE areas, to be laid out as 3 football pitches. The play area for reception year pupils is situated in the northern part of the site, between the classrooms and the hall and is accessed directly from the classrooms and is to be constructed of artificial play turf with mounding.
- 5.17 It is considered that these proposed facilities are satisfactory and will meet the operational needs of the school.

Noise

- 5.18 Condition 35 of the outline permission requires a scheme to be submitted to protect the school from noise from the operation of Gatwick Airport, including any future 2nd Runway. The scheme has to include the means by which the internal areas meet the required noise standards. It also requires evidence of reasonable efforts to achieve noise standards in an appropriately sized external teaching area, with regard to the siting and orientation of the buildings and the provision of a canopy over all or part of the external teaching area.
- 5.19 The level of noise intrusion will depend on the acoustic performance of all elements of the façade, with the ventilation and glazing being particular determining factors in the overall standard reached.
- 5.20 A noise report has been submitted with the application. The report indicates that the construction of the building will incorporate natural ventilation, with noise attenuated roof ventilators with summer boost ventilation by opening windows. A canopy over the southern external teaching area is also proposed to provide noise attenuation to this area.
- 5.21 Environmental Health and GAL Planning have advised that more noise data is required and detailed information of the specific noise mitigation measures (the ventilators) , together with information regarding the sound attenuating properties of the glazing, roofs and wall. Furthermore, with regard to the external teaching area, additional evidence of the reasonable efforts taken to achieve the required noise level are necessary.
- 5.22 At the time of writing this report the further information has yet to be submitted, however, it should be noted that the details required do not impact on the overall design or appearance of the building and these technical matters can be assessed in more depth through the discharge of condition request CR/1998/0039/CC20, which has been submitted in tandem with this reserved matters application.

Surface Water Drainage

- 5.23 The drainage strategy for the school would follow the principles established across the neighbourhood using sustainable drainage methods under condition 16 of the outline permission. However, a standalone approach for this site is proposed, as the measures previously modelled and agreed for the remainder of Phase 1 did not include capacity for the school. The school attenuation system has therefore been designed to be independent from the wider site storage /attenuation system. The scheme would incorporate a network of pipes and underground storage cells.
- 5.24 The Drainage Officer comments that the scheme follows good principles but additional calculations are required for the proposed network of pipes and storm cells. In addition, the drainage of the sports pitches is to be carried out by others and so additional information will be required to satisfy this element. Furthermore, detail regarding maintenance of the system is needed. It is proposed that these matters are covered by a specific condition.

Foul Drainage

- 5.25 Thames Water have commented that the existing waste water infrastructure is unable to accommodate the needs of the development and have recommended that a 'Grampian Style' condition be imposed requiring an on and off-site drainage strategy for the proposed works. However, it is not appropriate to impose a condition relating to strategic infrastructure to a reserved matter application as such issues should have been conditioned at outline stage. The appeal Inspector did not impose a specific condition in respect of foul water drainage.
- 5.26 The applicants have confirmed that they have a foul drainage strategy and agreement with Thames Water which covers the whole development which comprises 2 connections one each side of the Balcombe Road to serve the neighbourhood. .

Sustainability Measures

- 5.27 Condition 22 of the outline permission requires the non-residential buildings to achieve a BREEAM rating of 'very good'. The application is accompanied by a BREEAM report and a Sustainability

Overview statement. These indicate that the design of the school reflects the standard required by condition 22.

- 5.28 The key approach adopted is through the 'fabric first' concept to the construction of the development. In addition, regard has been had to future needs, with the design and layout allowing for a future wing of 7 classrooms to be added, if a 3FE school required. Furthermore, it is argued that the school should be seen in its wider context as an integral part of the new neighbourhood, which is being developed to provide a sustainable community.
- 5.29 In terms of the specific measures, the main elements are:
- external envelope, building acoustics and ventilation system designed to provide suitable noise environment given proximity to Gatwick Airport and location of future 2nd runway
 - high levels windows to achieve natural lighting and to reduce reliance on electric lighting
 - cooling through passive design and building orientation and glare controls to manage solar glare and sunlight
 - layout provides for views out from classrooms for 'wellbeing' credits
 - material chosen for low VOC's, from sustainable sources, BREEAM A rated
 - thermal modelling software used; energy use, heating and water consumption metered, water leak detection systems proposed, high efficiency gas boilers
 - cycle storage exceeds BREEAM criteria
 - landscaping to enhance ecology and wildlife habitat and species selected with low water consumption
 - attenuation tanks for surface water drainage to control flow discharge
- 5.30 Further information has been submitted on the specifics of some of the proposed proposals and it is now considered that the whole package of measures if implemented will meet the 'Very Good' rating.
- 5.31 In addition, the applicant has requested that the wording of condition 22 is revised to allow for the school to be occupied, but prior to the submission of the post construction certificate. However condition 22 is on the outline permission and relates to all the non- residential buildings within the Forge Wood neighbourhood. Thus it is considered that a specific condition for the school itself would be more appropriate, to reflect the circumstances relating to the provision of the school and can be imposed on this reserved matters approval.

Ecology, Landscaping and Trees

- 5.32 A detailed ecological report has been provided to support this application. In addition, an arboricultural report has been submitted. The site has been resurveyed on a number of occasions since the outline permission was submitted and the reports reflect this data. The Ecology Advisor has no objections to the application subject to the implementation of the protection and mitigation works for this site and as part of the wider development
- 5.33 The application site mainly comprises managed improved grassland, trees, scrub and a hedgerow. Currently the northern part of the site is used as the construction yard and site offices for the wider development.
- 5.34 There are no trees of any special merit on the application site itself, but to the immediate western boundary lies an area of mature trees, which are shown to be retained. The submitted plans include appropriate tree protection measures for these trees during construction works. The Arboricultural Officer has no objections to the application.
- 5.35 The application is accompanied by a detailed landscaping scheme, which has been revised following consultation with GAL regarding appropriate tree species for this location. In addition to tree and shrub planting and the soft and hard surfaced PE facilities, the school grounds would also include a shelter with seats, a meadow and wildflower lawn and vegetable garden/allotment area, a nature garden, bee hives and bulb planted areas.

- 5.36 It is considered that the submitted details are acceptable and subject to relevant conditions, would result in an attractive development.

Impact on nearby residents

- 5.37 Given the central location of the school within the Forge Wood development as a whole, the residents most affected will be those in the new dwellings in phase 1. In particular, occupiers in Somerley Drive and Hammingden Court.
- 5.38 This part of Forge Wood has always been identified for a school and was depicted as such on the approved Master Plan and in the Design Statement. Thus the principle of a school in close proximity to surrounding dwellings has been part of the overall concept for this mixed use neighbourhood.
- 5.39 Thus subject to the detail of the school proposals, there is no reason why a school would not be acceptable in such a location
- 5.40 Turning to the details, the comments from residents are acknowledged and whilst it is accepted that the indicative layout of the school buildings, playing fields and access arrangements differ from that previously shown, it should be noted that those elements were not approved as part of the outline permission, but were to form reserved matters for future detailed assessment. The same would apply to the other parcels of the site, such as the residential phases.
- 5.41 In this case, the design and layout of the school site has evolved since the date of the original permission having regard to the relationship with the surrounding context, especially the local centre as now approved and considerations of access (both pedestrian and vehicular), outdoor play and learning spaces, security and the need to provide a suitable learning environment for pupils given the proximity to Gatwick Airport.
- 5.42 The school is positioned centrally within the plot, with distances at the closest point of 45m to the dwellings on the eastern side of Somerley Drive and 38m to Hammingden Court. With these separation distances and the single storey scale of the design it is not considered that the building would be overbearing or give rise to issues of overlooking or privacy. The 'right to a view' is not a planning consideration.
- 5.43 WSCC Highways have no objections to the application on highway safety grounds and it is not considered that the access and egress arrangements as proposed on the main link road through the neighbourhood would be so detrimental to the amenities of nearby residents to warrant a refusal on the grounds of noise or disturbance from traffic.
- 5.44 With regard to the visual impact of the accesses and car parking, the landscaping proposals include a beech hedge along the eastern frontage to Somerley Drive with tree planting, together with 1.8m high bow top metal fencing. These measures are considered to be acceptable and provide an appropriate solution from a visual and security point of view.

CONCLUSIONS:-

- 6.1 This application is for a further element of Phase 1 of the Forge Wood development and seeks agreement to the siting, design and external appearance of the primary school. It is considered that this application has addressed the design parameters on the outline permission, is in substantial accordance with the Master Plan and addresses the relevant conditions imposed.
- 6.2 The contemporary design approach is considered to be appropriate for a school in this location and the layout has had regard to the context of the Local Centre, the proximity to Gatwick Airport and the operational requirements for the school.
- 6.3 The layout in terms of its design, access arrangements, landscaping and indicative palette of materials provides for an attractive and legible layout and building. The layout is considered to appropriately

address the environmental safeguards set out in the outline conditions in terms of ecology, noise and drainage, subject in some cases to further work to discharge the relevant conditions.

6.4 The application has been considered in relation to the wider requirements on the outline permission, and notwithstanding that some of the conditions and details are yet to be fully discharged, the approval of the design, layout and external appearance of the school would not compromise the requirements of these conditions and their eventual discharge, subject to the appropriate level of detail.

6.5 It is therefore recommended to APPROVE the reserved matters for the primary school as set out below:

RECOMMENDATION RE: CR/2016/0048/ARM

APPROVE: subject to the following conditions

1. The development hereby permitted shall not be carried out other than in accordance with the approved plans as listed in this Decision Notice save as varied by the conditions hereafter.
REASON: For the avoidance of doubt and in the interests of proper planning.
2. The development hereby permitted shall not be occupied until the accesses, parking spaces and turning facilities shown on the submitted plans have been provided and constructed in accordance with the approved details. The areas of land so provided shall not thereafter be used for any purpose other than the parking and turning of vehicles.
REASON: To ensure that adequate and satisfactory provision is made for the accommodation of vehicles clear of the highways in accordance with Policy CH3 of the Crawley Borough Local Plan 2015 – 2030.
3. The development hereby permitted shall not be occupied unless and until provision for the parking of cycles and scooters has been made within the site in accordance with the approved planning drawings. The facilities so provided shall be retained solely for that purpose.
REASON: To ensure the adequate provision for the parking of cycles and scooters in accordance with policy CH3 of the Crawley Borough Local Plan 2015 – 2030.
4. The school building hereby approved shall not be occupied unless and until school safety zone measures have been installed on the new spine road outside the school comprising of yellow keep clear markings, flashing school warning signs and appropriate dropped crossing points in accordance with plans and details to be approved by the Local Planning Authority.
REASON: in the interests of highway safety in accordance with policy CH3 of the Crawley Borough Local Plan 2015 – 2030.
5. Within 3 months of the school building being occupied, a School Travel Plan shall be prepared and submitted for the approval of the Local Planning Authority to encourage pedestrian and cycle access to the school. The Travel Plan shall be completed in accordance with the latest guidance and good practice documentation as published by the Department for Transport or as advised by the Highway Authority. The Travel Plan within one month of its approval shall be implemented as specified within the approved document.
REASON: In the interests of highway safety, to reduce the need for private car travel to the school and to encourage and promote sustainable transport, in accordance with policies CH3 and IN4 of the Crawley Borough Local Plan 2015- 2030.
6. Prior to the commencement of the development or any preparatory work hereby approved, retained trees shall be protected in accordance with BS5837:2012, the details described within the submitted Arboricultural Report(ref JTK/9126/so) and the Tree Protection Plan - drawing number L(SK)04 Rev B.
REASON: To maintain the long term health of the trees which are important to the site in accordance with policy CH3 of the Crawley Borough Local Plan 2015 – 2030.

7. The construction of the development shall be undertaken in accordance with the safeguards detailed in the submitted Ecological Assessment – Forge Wood Primary School dated January 2016 provided by Ecology Solution Ltd.

REASON: to ensure that the ecology of the site is protected and enhanced in accordance with policy ENV2 of the Crawley Borough Local Plan 2015 – 2030 and para 116 of the NPPF 2012.

8. Development shall not commence until a Bird Hazard Management Plan has been submitted to and approved by the Local Planning Authority. The submitted plan shall include details of:

Management of any flat/shallow pitched/green roofs on the proposed school which may be attractive to nesting, roosting and 'loafing' birds. The management plan shall comply with Advice Note 8 'Potential Bird Hazards from Building Design' (available at www.aoa.org.uk/policy-campaigns/opertaions-safety/)

The Bird Hazard Management Plan shall be implemented as approved, upon completion of the roof and shall remain in force for the life of the building. No subsequent alterations to the plan are to take place unless first submitted to and approved in writing by the Local Planning Authority.

REASON: In the interests of the environment of the development and to mitigate bird hazard and avoid endangering the safe movement of aircraft and the operation of Gatwick Airport through the attraction of birds, in the accordance with Policy CH3 of the Crawley Borough Local Plan 2015 – 2030.

9. The soft landscaping shall be implemented in full prior to the end of the first planting and seeding season following the occupation of the building or the completion of the development, whichever is the sooner, in accordance with the following details:

- Landscape Master Plan – drawing number L(SK)01 Rev B
- Preliminary Planting Plan – drawing number L(SK)02 Rev B

Any trees or plants which within a period of five years from the completion of the development die, are removed, or become seriously damaged or diseased shall be replaced in the next planting season with others of similar size and species, unless the Local Planning Authority gives written consent to any variation.

REASON: In the interests of amenity and of the environment of the development and because the scheme has been designed to mitigate bird hazard and avoid endangering the safe movement of aircraft and the operation of Gatwick Airport through the attraction of birds, in the accordance with Policy CH3 of the Crawley Borough Local Plan 2015 – 2030.

10. The hard landscaping and boundary treatment for the site shall be implemented prior to the occupation of the building or the completion of the development, whichever is the sooner, in accordance with the following details:

- Preliminary Hardscape Plan - drawing number L(SK)03 Rev B
- Boundary Treatments Plan - drawing number 31985_LL(90)02 Rev A
- Landscape Master Plan – drawing number L(SK)01 Rev B

REASON: In the interests of the visual amenity of the development and to achieve a development of visual quality in accordance with policies CH3 of the Crawley Borough Local Plan 2015 - 2030.

11. Before the school hereby permitted is occupied, details of the design, location and luminance levels of the external lighting and floodlighting shall be submitted to and approved by the Local Planning Authority, and thereafter implemented in accordance with the agreed details.

REASON: In the interests of the amenities of the locality in accordance with Policy CH3 of the Crawley Borough Local Plan 2015 – 2030.

12. The development hereby permitted shall not proceed until full details of the proposed surface water drainage and means of disposal relating to the school building, grounds and sports pitches have been submitted to and approved by the Local Planning Authority. The details shall include a programme for implementation and proposals for the subsequent management and maintenance of the drainage system. The development shall be carried out in accordance with the approved details.

REASON: To ensure that the proposed development is satisfactorily drained in accordance with Policy ENV8 of the Crawley Borough Local Plan 2015 – 2030.

13. The development hereby permitted shall be implemented in accordance with the BREEAM report, Sustainability Overview Statement and further clarification (letter from agent dated 20th May 2016) as detailed in the application.
Within three months of the completion of the building a post-construction report shall be submitted to the Local Planning Authority for approval verifying that the development has achieved a 'very good' rating, unless the Local Planning Authority gives written consent to any variation.
REASON: in the interests of sustainable development in accordance with policies ENV6 and ENV9 of the Crawley Borough Local Plan 2015 - 2030

INFORMATIVES

1. The applicants will be required to fund any necessary traffic regulation orders in order to support the school safety zone.
2. Given the nature of the proposed development it is possible that a crane may be required during its construction. We would, therefore, draw the applicant's attention to the requirement within the British Standard Code of Practice for the safe use of Cranes, for crane operators to consult the aerodrome before erecting a crane in close proximity to an aerodrome. Gatwick Airport requires a minimum of four weeks' notice. For crane queries/applications please email gal.safeguarding@gatwickairport.com. The crane process is explained further in Advice Note 4, 'Cranes and Other Construction Issues' (available at <http://www.aoa.org.uk/operations-safety/>).
3. Surface Water Drainage - With regard to surface water drainage it is the responsibility of a developer to make proper provision for drainage to ground, water courses or a suitable sewer. In respect of surface water it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of groundwater. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0800 009 3921.
REASON - to ensure that the surface water discharge from the site shall not be detrimental to the existing sewerage system.

NPPF Statement

1. In determining this planning application, the Local Planning Authority assessed the proposal against all material considerations and has worked with the applicant in a positive and proactive manner based on seeking solutions where possible and required, by:
- Providing advice through pre-application discussions/correspondence.
 - Liaising with consultees/respondents/applicant/agent and discussing the proposal where considered appropriate during the course of the determination of the application.
 - Seeking amended plans/additional information to address identified issues during the course of the application.

This decision has been taken in accordance with the requirement in the National Planning Policy Framework, as set out in article 35, of the Town and Country Planning (Development Management Procedure) Order 2015.



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CR/2016/0048/ARM

Date 3 May 2016

Approx. Scale 1:1,250

PHASE 1, FORGE WOOD, (NORTH EAST SECTOR), CRAWLEY



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REFERENCE NO: CR/2016/0109/FUL

LOCATION: [5 BARNWOOD, POUND HILL, CRAWLEY, RH10 7TH](#)
PROPOSAL: ERECTION OF TWO STOREY SIDE EXTENSION

TARGET DECISION DATE: 31 March 2016

CASE OFFICER: Miss D. Angelopoulou

APPLICANTS NAME: Mr J Small
AGENTS NAME: JNA Architects

PLANS & DRAWINGS CONSIDERED:

J1190-1 RevA Location Plan, Existing Elevations, Existing Roof Plan, Existing Ground and First Floor Plans, J1190-02 RevB Block Plan, Proposed Elevations, Proposed Roof Plan, Proposed Ground and First Floor Plans, Planning Statement (Incorporating Design & Access Statement)

CONSULTEE NOTIFICATIONS & RESPONSES:-

- | | | |
|----|--|--|
| 1. | CBC - Planning Arboricultural Officer | No objection |
| 2. | Archaeology Officer - Alex Egginton | No Objection; Site is Archaeological Notification Area due to the Pound Hill Medieval Moated site and surrounding earthworks. An assessment of the archaeological implications of the proposed development not provided, however, the extension does not appear to impact earthwork features. |
| 3. | Forward Planning ASEQ – Urban Design Officer | Objection – Application should be supported by Heritage Impact Assessment; Proposed projection beyond the existing front building line is inappropriate, need to maintain a spacious feel, proposed raised roof ridge would alter the building's character and that of the wider area, eastern flank is unacceptable |

NEIGHBOUR NOTIFICATIONS:-

6, 17 and 18 Barnwood, Pound Hill; 19, 21, 23 Worth Park Avenue, Pound Hill.

RESPONSES RECEIVED:-

One representation has been received from nearby occupier raising objections that the extension would be unsympathetic to the street scene and negative impact resulting from the additional roof height.

REASON FOR REPORTING TO COMMITTEE:-

Councillor Robert Lanzer requested to include the application on Planning Committee agenda.

THE APPLICATION SITE:-

- 1.1 The application site relates to a detached bungalow style property situated on the north east side of Barnwood Close within the neighbourhood of Pound Hill. The property is within an Area of Special Local Character (ASLC) as designated by the Crawley Borough Local Plan 2015-2030 and within an Archaeological Notification Area (ANA) as designated due to the Pound Hill Medieval Moated Site. It is also approximately 100 metres from the Scheduled area of the moated site. There are significant gaps between most buildings in the area and mature landscaping giving the area a spacious character. Barnwood Close is characterised by homogenous and cohesive character, historic landscape features, landscape value e.g. mature trees, hedges, grass verges and low density housing in a spacious landscaped setting.
- 1.2 The property is set in a spacious plot and to the front there are 2 protected Sweet Chestnut trees. The property is finished in render and the roof is finished in a plain clay tile. The property currently has a pitch roofed front dormer and a large flat roofed rear dormer. The garden falls away to the rear and is surrounded by fencing and hedges. The driveways serving the property have space to accommodate 2 vehicles.

THE PROPOSED DEVELOPMENT:-

- 2.1 Planning permission is sought for the erection of two storey side extension, which would be positioned on the north-western side of the dwelling. It would measure approximately 5 metres in width and would protrude beyond the existing front building line of the dwelling by 0.25 metres. The proposal would measure 8.4 metres in depth and would project approximately 0.9 metres from the existing rear building line. It would have hipped roof and would feature a roof ridge which would be raised 0.5 metres above the existing to facilitate first floor room heights. Following internal alterations, it would form an enlarged kitchen/dining area on ground floor and an additional master bedroom with ensuite shower and dressing room on first floor. The front of the extension would contain two ground floor windows and a first floor window, the rear elevation would contain bifold doors and first floor window and the side elevation would contain one door. The extension would be constructed in materials to match the design of the existing dwelling, however it would introduce aluminium windows and doors.

PLANNING HISTORY:-

- 3.1 None relevant to this application.

PLANNING POLICY:-

National Planning Policy Framework (2012) (NPPF)

- 4.1 The NPPF states that the applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. More specifically:
 - Paragraph 14 – Presumption in favour of sustainable development. At the heart of the NPPF is a presumption in favour of sustainable development, which should be seen as a golden thread running through both plan-making and decision-taking.
 - Paragraph 17 – Core planning principles. Always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings.
 - Section 7 – Requiring good design. The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.

Crawley Borough Local Plan (2015-2030) (adopted December 2015)

- 4.2 The relevant policies include:

- Policy SD1: Presumption in Favour of Sustainable Development. In line with the planned approach to Crawley as a new town, and the spatial patterns relating to the neighbourhood principles, when considering development proposals the council will take a positive approach to approving development which is sustainable.
- Policy CH2: Principles of Good Urban Design in order to assist in the creation, retention or enhancement of successful places.
- Policy CH3: Normal Requirements of All New Development states all proposals for development in Crawley will be required to make a positive contribution to the area, be of a high quality design, provide and retain a good standard of amenity for all nearby and future occupants of land and buildings, and be able to meet its own operational requirements necessary for the safe and proper use of the site.
- Policy CH12: Heritage Assets states that all development should ensure that Crawley's designated and non-designated heritage assets are treated as a finite resource, and that their key features or significance are not lost as a result of development. Where a development affects a heritage asset or the setting of a heritage asset, a Heritage Impact Assessment will be required.
- Policy CH14: Areas of Special Local Character (ASLC) states that all development within an ASLC should respect or preserve the character of the area and have been designed with regard to the areas existing character and appearance. Proposals should be of an appropriate scale, design and massing, and should not result in significant adverse impact on the locality, its surrounds and vistas. All development within an ASLC should demonstrate, as part of the Heritage Impact Assessment, how the proposals have regard to the reasons for the area's designation and the character and appearance of the area.

Supplementary Planning Guidance Note: 5 – Residential Extensions (2001)

- 4.3 Supplementary Planning Guidance Note 5 '*Residential Extensions*' is a non-statutory document which supplements the policies of the Local Plan and is applicable to this application. It contains guidelines on the two storey side extensions and in particular, it states: '*As with a front extension, an extension on the side of your house will be prominent as viewed from the street. It is therefore important that it should fit in with its surroundings. The filling up of the gaps between houses by the construction of two storey side extensions can make detached or semi-detached houses in a street look like terraced houses. This can change the character of a street quite dramatically and is known as 'terracing'. In areas characterised by spacious houses surrounded by large gardens, a larger gap between the boundary and extension may be required in order to avoid a detrimental effect on the street scene. Ensure that detailing and materials of an extension matches that of the original building. Avoid windows that directly overlook your neighbour's garden. Where unavoidable, high level windows or obscure glass windows may be acceptable.'*

Supplementary Planning Document 2008 – Planning Obligations and S106 Agreements

- 4.4 The Annex B of this Supplementary Planning Document is also relevant and contains guidelines on the adopted parking standards, such as the maximum parking standards for a 3 plus bedroom dwelling in this location are 2-3 spaces.

PLANNING CONSIDERATIONS:-

- 5.1 The main planning issues in the determination of this application are:
- The design & appearance of the proposal and its impact on the dwelling, street scene & character of ASLC
 - The impact on neighbouring properties and amenities
 - Parking considerations
 - Impact on protected trees

The design & appearance of the proposal and its impact on the dwelling, street scene & character of ASLC

- 5.2 The proposed two storey side extension would slightly project in front of the existing dwelling. The existing dwelling features a hipped roof with a low pitch and is set spaciouly away from the chimney core on the eastern flank. The proposed extension would have both significantly higher ridge and eaves height and in order to connect to the original dwelling and requires alterations to the original roof pitch resulting in greater encroachment on the chimney core at the eastern end. The proposed overall massing, size and changes to the roofscape of the dwelling are considered to dramatically alter the dwelling's character. According to Urban Design Officer's comments regarding the eastern flank façade: *'The proposed roof design will create an awkward and disproportionate appearance on the dwelling's eastern flank, partly due to the height of the raised ridge and partly due to the overall design of the proposal. The proposed eastern flank is unacceptable and should be resolved through amending the plans'*.
- 5.3 Furthermore, the introduction of a gable feature is considered incongruous and to dominate the front elevation of the property and detract from the original chalet bungalow design features of the property. The resultant development changes the character of the building from a chalet bungalow to 2 storey dwelling and removes from the original design the important features which give the dwelling character.
- 5.4 Therefore, it is considered that the proposed two storey side extension would, by virtue of size, massing and design, detract from the character of the original property and would be contrary to Policy CH3 of the Crawley Borough Local Plan 2015-2030, the design guidance set out in the Supplementary Planning Guidance Note 5: Residential Extensions (2001) and the relevant paragraphs of the National Planning Policy Framework (2012).
- 5.5 In respect to the impact of the development on the character of the area, the Crawley ASEQ and Locally Listed Building and Heritage Assessment 2010 identifies the existing spacing and positioning of the dwellings in their plots as an important feature to the area. It is considered that the two-storey side extension and in particular its forward projection, resultant scale and overall depth would detract from the spacious character. The proposed two storey side extension would be particularly visible from Worth Park Avenue and the entrance to Barnwood and incongruous feature on approach to the area. With regards to the prominent western flank, whilst it is acknowledged that the proposed two storey side extension would obstruct the view of the front and rear dormers, which are considered to be out of character to the area, however it is considered that the western flank façade due to its significant public views would not relate sympathetically and symmetrically to the existing dwelling and the character of the area.
- 5.6 Therefore, the extent of the projection to the front, the introduction of a gable, the increase in the height of the eaves and the ridge of the roof combined with the overall design of the proposal, the introduction of aluminium materials for the windows and doors make the proposed two storey side extension a dominant feature in the street which is not replicated elsewhere, in this area of predominantly single storey developments with accommodation in the roof-space retaining characteristic low eaves heights. It is also considered that the prominence of the proposed two storey side extension would be out of keeping with the street scene and the character of ASLC which would therefore clearly have an adverse impact on the character and appearance of the area and would be contrary to the aims of protecting the ASLC as set out in policy CH14 and also in the general policies CH2, CH3 and CH12 of the Crawley Borough Local Plan 2015-2030.
- 5.7 During the course of the determination of the application the applicant was made aware of the design concerns and the required amendments through a meeting, but requested the application to be determined as submitted.

The impact on neighbouring properties and amenities

- 5.8 In terms of the impact on neighbouring properties, the properties most affected are No.21 Worth Park Avenue to the north-west and No.23 Worth Park Avenue to the north.
- 5.9 Regarding No. 21 Worth Park Avenue, the proposed two storey side extension would only have a door on its side elevation and given the separation distance, it is not considered that it would have

any detrimental impact on the amenities enjoyed by the occupants of this neighbouring properties in terms of overlooking or overbearing presence.

- 5.10 With regards to No. 23 Worth Park Avenue there would be a retained gap of approximately 18.5 metres between the extension and the side elevation of this neighbouring property. The proposed two storey side extension would have a first floor rear window which would face the neighbour's front garden and side elevation with ground floor windows which serve a bedroom and lounge room. These ground floor windows are screened by bushes and given the separation distance, it is considered that the proposed two storey side extension would not cause unreasonable harm to the amenity of this neighbouring property in terms of overlooking.
- 5.11 As a result, the proposed two storey side extension would not have any detrimental impact on the amenities enjoyed by the occupants of neighbouring properties and would accord with the Local Plan Policies, SPG 5 and the NPPF.

Access and parking considerations

- 5.12 The proposal would create an additional bedroom resulting in the property becoming a 4 bedroom dwelling. The driveways serving the property have space to accommodate 2 vehicles. The maximum parking standards for a 3 plus bedroom dwelling in this location are 2-3 spaces and the parking arrangements are therefore considered satisfactory and would accord with the Policy CH3 and the guidance set out in the Planning Obligations and S106 Agreements Supplementary Planning Document 2008.

Impact on protected trees

- 5.13 To the south-east side of the application site there are 2 protected Sweet Chestnut trees under Barnwood Close No.1 TPO 1948, which, according to the Planning Arboricultural Officer, they would not be affected by the extension.

CONCLUSIONS:-

- 6.1 For the reasons outlined above, the proposed two storey side extension would be out of keeping with the existing dwelling, the street scene and the character of the Area of Special Local Character and would fail to accord with the National Planning Policy Framework (2012) and the Policies of the Crawley Borough Local Plan 2015-2030, and it is therefore recommended for refusal.

RECOMMENDATION RE: CR/2016/0109/FUL

REFUSE - For the following reasons:-

1. The proposed two storey side extension would, by virtue of size, massing and design, detract from the character of the original property and would be contrary to Policy CH3 of the Crawley Borough Local Plan 2015-2030, the design guidance set out in the Supplementary Planning Guidance Note 5: Residential Extensions (2001) and the relevant paragraphs of the National Planning Policy Framework (2012).
2. The proposed two storey side extension would, by virtue of its siting, scale and design, result in a building which would be out of keeping with the character of development in the street. The proposed two storey side extension would therefore be detrimental to the street scene and would cause harm to the established character of the Area of Special Local Character contrary to Policies CH2, CH3, CH12 and CH14 of the Crawley Borough Local Plan 2015-2030, the design guidance set out in the Supplementary Planning Guidance Note 5: Residential Extensions (2001) and the relevant paragraphs of the National Planning Policy Framework (2012).

NPPF Statement

1. In determining this planning application, the Local Planning Authority assessed the proposal against all material considerations and has worked with the applicant in a positive and proactive manner based on seeking solutions where possible and required, by:
 - Liaising with agent and discussing the proposal where considered appropriate and necessary in a timely manner during the course of the determination of the application.
 - Seeking amended plans to address identified issues during the course of the application.
 - Informing the applicant of identified issues that are so fundamental that it has not been possible to negotiate a satisfactory way forward due to the harm that would be caused.

This decision has been taken in accordance with the requirement in the National Planning Policy Framework, as set out in article 35, of the Town and Country Planning (Development Management Procedure) Order 2015.



Crawley Borough Council,
Town Hall,
The Boulevard,
Crawley,
West Sussex RH10 1UZ

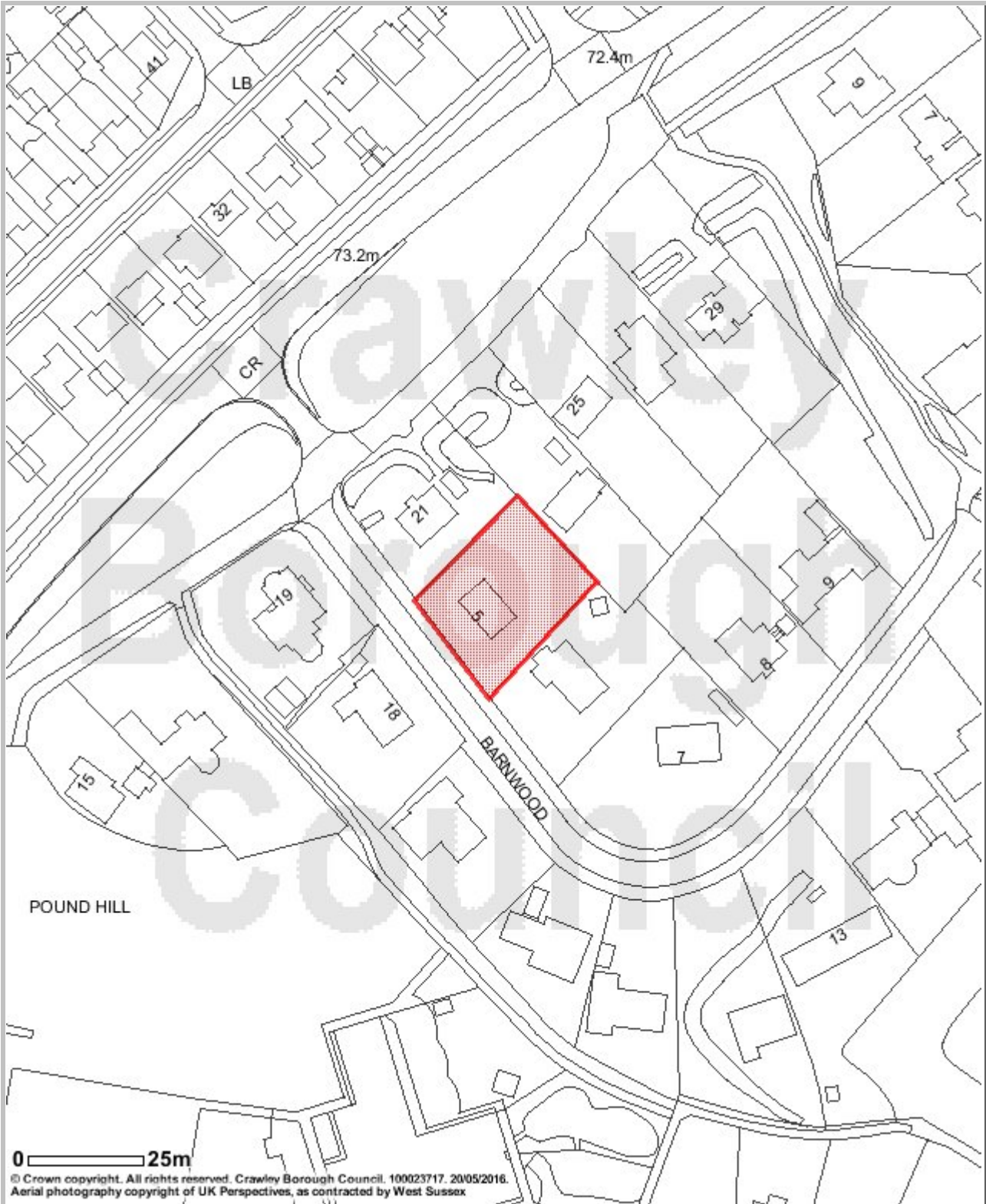
Tel: 01293 438000
Fax: 01293 438603

CR/2016/0109/FUL

Date 17 May 2016

Approx. Scale 1:1,250

**5 BARNWOOD, POUND HILL, CRAWLEY, RH10
7TH**



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REFERENCE NO: CR/2016/0115/FUL

LOCATION: [23 ASH ROAD, THREE BRIDGES, CRAWLEY, RH10 1SG](#)

PROPOSAL: GARAGE CONVERSION TO HABITABLE ACCOMMODATION & PROVISION OF 3RD PARKING SPACE TO FRONT

TARGET DECISION DATE: 4 April 2016

CASE OFFICER: Ms E. Wachiuri

APPLICANTS NAME: Mr Steve Davies

AGENTS NAME: Heritage Asset Management

PLANS & DRAWINGS CONSIDERED:

AR-2016-01 Existing & Proposed Floor Plans & Elevations

CONSULTEE NOTIFICATIONS & RESPONSES:-

1. CBC - Housing Strategic Service (HMO's)

NEIGHBOUR NOTIFICATIONS:-

21, 25, 42, 44, 46 Ash Road, Three Bridges

RESPONSES RECEIVED:-

Five objections received which raised the following concerns:

- The house is in multiple occupation and thus would need 4 parking spaces and not 3 as proposed
- The proposed 3rd parking space would deprive other road users from using the parking space directly outside the application house as the new parking space would not be able accessible directly from the existing driveway
- The proposal due to multiple occupancy of the property would put pressure on the existing on-street parking which is unfair to other residents
- The house is in multiple occupancy of six separate units. If garage is converted, there will only be 3 off-street spaces and the rest will rely on on-street parking inconveniencing other residents.
- Has the property got planning permission or licence from the Council for multiple occupancy?

REASON FOR REPORTING TO COMMITTEE:-

Because of the number of objections received and the recommendation to permit.

THE APPLICATION SITE:-

- 1.1 This relates to a semi-detached house located on the west side of Ash Road. **The property has been converted into a house in multiple occupation (HMO) providing six bedrooms with shared kitchen/diner, lounge and bathroom facilities.**
- 1.2 The property currently benefits from 2 parking spaces on the front driveway and a single integral garage.
- 1.3 There are several lay-bys for parking in the surrounding areas along Ash Road.

THE PROPOSED DEVELOPMENT:-

- 2.1 Planning permission is sought for the conversion of the existing integral garage into habitable accommodation (bedroom) and the provision of a 3rd replacement parking space in the front garden.
- 2.2 The garage conversion would provide an additional bedroom whilst the smallest of the existing bedrooms would be used as a study/store as indicated on the submitted drawing reference AR-2016-01. The property would thus remain as a 6 bedroom HMO. The garage door would be replaced with a brick wall and window above. The materials would match the existing.
- 2.3 An additional off-street car parking space on the front garden is also being proposed to replace the space lost by the garage conversion.

PLANNING HISTORY:-

Most relevant is:

- 3.1 CR/071/1989 – Erection of two storey side extension. Condition 3 states that provision shall be made within the site for the parking/garaging of three motor vehicles to the satisfaction of the Local Planning Authority. Such provision shall thereafter be used only for the parking of motor vehicles.
- 3.2 CR/357/1978 – Permission granted for a single storey rear extension

PLANNING POLICY:-

4.1 National Planning Policy Framework (2012):

- Paragraph 17 (Core planning principles). Always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings.
- Chapter 59 and 60 (Requiring good design). The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.

4.2 Crawley Borough Local Plan 2015-2030:

- Policy CH2 (Principles of Good Urban Design) States that all proposals for development in Crawley will be required to respond and reinforce local distinctive patterns of development and landscape character, and create continuous frontages onto streets and spaces enclosed by development which clearly defines private and public areas.
- Policy CH3 (Normal Requirements of All Development) states all proposals for development in Crawley will be required to make a positive contribution to the area; be of a high quality design, provide and retain a good standard of amenity for all nearby and future occupants of land and buildings, retain existing individual or groups of trees that contribute positively to the area and be able to meet its own operational requirements necessary for the safe and proper use of the site.
- Policy IN4 (Car and Cycle Parking Standards) states development will be permitted where the proposals provide the appropriate amount of car and cycle parking to meet its needs when it is assessed against the borough council's car and cycle parking standards.

4.3 Supplementary Planning Guidance

- Planning Obligations and s106 Agreements - Supplementary Planning Document 2008 (SP: Annex B contains the adopted parking standards. It includes the Council's maximum parking standards. Houses in Multiple Occupation have a maximum parking requirement of 0.5 spaces per unit.

- Residential Extensions - Supplementary Planning Guidance Note: 5 (2001) Contains guidelines on the standards the Council expects for the design of extensions and improvements.
- The provisions of the Housing Acts 1985 and 2004 are also a material consideration in this case as the Local Plan has no detailed guidance on such standards for assessing planning applications for HMO's.

PLANNING CONSIDERATIONS:-

- 5.1 The main considerations in determining this application relate to:
- The impact of the proposed alterations on the character of the existing property, street scene and character of the area;
 - The impact of the proposal on the amenities of neighbouring occupiers;
 - Parking;
 - Use of the resultant building;
 - The acceptability of the accommodation for future occupiers;

The design and appearance of the resultant building and its impact upon the character of the street scene/visual amenities of the area;

- 5.2 To the front of the property, the garage door would be replaced with a window and a brick wall to match the existing. These alterations to the front of the house are considered to be modest changes to the existing building that would not have an adverse impact on visual amenity, the street scene or the character of the area. The proposal is therefore considered acceptable and would not have an adverse impact on visual amenity, the street scene or the character of the area.

Impact on neighbouring amenities;

- 5.3 Apart from replacing the garage door with a brick wall and window above, there would be no other physical external alterations and no addition volume created as a result of the proposal. The alterations would therefore not result in any loss of privacy nor have harmful impacts on neighbour amenities. With regard to the use, no intensification of use will occur as will still be a 6 bedroom HMO and so no additional traffic and activity. It is therefore considered that the proposed garage conversion would have an acceptable impact on neighbouring occupiers' amenities in accordance with policy CH3 of the Crawley Borough Local Plan 2015-2030

Parking;

- 5.4 The proposed development would result in the loss of one parking space within the garage. However, an additional parking space would be provided in the front garden as a replacement resulting in three parking spaces to the front of the property. According to SPD1, Houses in Multiple Occupation have a maximum parking requirement of 0.5 spaces per unit and in this case 3 parking spaces would be required. Therefore, the proposed three parking spaces provision accords with the Council's parking standards for a six bed house in multiple occupation. In addition, there are several but non-dedicated parking spaces in lay-bys along Ash Road and therefore planning permission cannot be refused on the grounds of loss of a parking space which forms one of the issues raised by objection to this application.
- 5.5 There are concerns that the additional parking space might require the widening of the existing vehicular crossover to enable a direct access from the Ash Road leading to further loss of on-street parking on this street. Since, Ash Road is a non-classified Road the widening of the vehicle crossover would not require planning permission in isolation. Also, the parking space would be constructed of a permeable surface, and this also can be constructed without the need for planning permission. Therefore planning permission cannot be refused on these grounds alone.
- 5.6 However, any widening of the crossover would still require a licence from the Highways Department which is a non-planning issue and the applicant would be advised of this in an informative.

Use of the resultant building:

- 5.7 The other concern relates to the future use of the property as a house in multiple occupancy with more than six occupants resulting in noise and parking problems. However, the submitted plans show only 6 bedrooms and so there is nothing to suggest the resultant dwelling house would be put into unauthorised use.
- 5.8 Furthermore, according to the Town and Country Planning (General Permitted Development) Order 2015 the change of use from a dwellinghouse (C3) to a small house in multiple occupation (C4) (occupied by 3 to 6 persons) is permitted development and therefore would not require planning permission. However, in order to control the level of occupants of the resultant development, a restrictive condition limiting the level of occupancy to a maximum of six persons is recommended. Should a breach occur in the future, then appropriate action may be taken at that time.
- 5.9 With regards to concern raised as to whether the proposal has a HMO licence, this is not a planning issue and does not form part of the planning considerations.

CONCLUSIONS:-

- 6.1 The proposed garage conversion is considered to be acceptable and would not detract from the character and appearance of the application property or the surrounding area in general. Nor would it be harmful to the amenities of the neighbouring residents. As such, the proposed development would comply with the relevant policies of the Core Strategy (2008), the saved Local Plan (2000) and the guidance contained within Supplementary Planning Guidance Note 5 on 'Residential Extensions' and the Supplementary Planning Document on Planning Obligations and S106 Agreements

RECOMMENDATION RE: CR/2016/0115/FUL

- 7.1 Permit subject to the following conditions.
1. The development hereby permitted shall be begun before the expiration of 3 years from the date of this permission.
REASON: To comply with Section 91 of the Town & Country Planning Act 1990.
 2. The development hereby permitted shall not be carried out other than in accordance with the approved plans as listed in this Decision Notice save as varied by the conditions hereafter.
REASON: For the avoidance of doubt and in the interests of proper planning.
 3. The materials and finishes of the external walls of garage conversion hereby permitted shall match in colour and texture those of the existing building(s).
REASON: In the interests of amenity in accordance with Policies CH3 of the Crawley Borough Local Plan 2015-2030.
 4. The use hereby permitted shall not be occupied until provision for a 3rd car parking space has been made within the site in accordance with approved plans reference AR-2016-01. The car parking facilities shall then be maintained and shall not be used for any other purpose other than the parking of cars.
REASON: To ensure that adequate and satisfactory provision is made for the parking of vehicles in accordance with Policies CH3 and IN4 Crawley Borough Local Plan Crawley 2030 and SPD 'Planning Obligations and S106 Agreements'.
 5. The internal layout and use of the rooms herein, of the house in multiple occupation hereby approved, shall be retained in accordance with the floor plans of the approved drawing AR-2016-01 Existing & Proposed Floor Plans & Elevations and shall not be altered in any way without the submission of an application on that behalf.

REASON: To enable the Local Planning Authority to control the use of the house in multiple occupation in detail to avoid an over-intensive use of the site in accordance with Policy H6 of the Crawley Borough Local Plan Crawley 2030.

6. The 6 bedroom house of multiple occupation hereby permitted shall be occupied by no more than six persons (1 person per room).

REASON: To enable the Local Planning Authority to control the use of the house in multiple occupation in detail and to avoid an over-intensive use of the site in accordance with policy H6 of the Crawley Borough Local Plan Crawley 2030.

INFORMATIVE

1. The applicant is advised to contact the West Sussex Area Highways Officer covering the respective area (01243 642105) to obtain formal approval from the highway authority to carry out any site access works on the public highway.

NPPF STATEMENT

1. In determining this planning application, the Local Planning Authority assessed the proposal against all material considerations and has worked with the applicant in a positive and proactive manner based on seeking solutions where possible and required, by:

Providing advice in a timely manner through pre-application discussions/correspondence.

Liaising with applicant/agent and discussing the proposal where considered appropriate and necessary in a timely manner during the course of the determination of the application.

Seeking amended plans/additional information to address identified issues during the course of the application.

This decision has been taken in accordance with the requirement in the National Planning Policy Framework, as set out in article 35, of the Town and Country Planning (Development Management Procedure) Order 2015.



Crawley Borough Council,
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Tel: 01293 438000
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CR/2016/0115/FUL

Date 22 April 2016

Approx. Scale 1:1,250

**23 ASH ROAD, THREE BRIDGES, CRAWLEY,
RH10 1SG**



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REFERENCE NO: CR/2016/0166/FUL

LOCATION: [56 - 60 NORTH ROAD, THREE BRIDGES, CRAWLEY](#)

PROPOSAL: DEMOLITION OF THREE HOUSES AND ERECTION OF 24 (14 X 2 BEDROOM & 10 X 1 BEDROOM) APARTMENTS, ACCESS, PARKING AND ASSOCIATED LANDSCAPING.

TARGET DECISION DATE: 24 May 2016

CASE OFFICER: Mr G. Kellett

APPLICANTS NAME: Places for People Homes Ltd

AGENTS NAME: GHM Partnership Ltd

PLANS & DRAWINGS CONSIDERED:

Location Plan 100, Demolition Plan 116, Existing Site Plan 101, Proposed Drainage Strategy C85268-D-001 A, Surface Water Drainage Assessment C85268/R001C, Proposed Site Plan 102 B, Proposed Landscaping 103 B, Boundary Treatment Plan 104 B, Plots 1-20 Ground Floor GF Plan 3141 105 B, Plots 1-20 First Floor Plan 3141 106 B, Plots 1-20 Second Floor Plan 3141 107 B, Plots 1-20 Roof Plan 3141 108 B, Existing and Proposed Street Scenes 3141 112 B, Access and Movement Plan 3141 113 B, Refuse Strategy Plan 3141 114 B, Open Space Plan 3141 115 B, Plots 1-20 Elevations 3141 109 C, Plots 21-24 GA Plans and Elevations 3141 110 Rev A, Cycle and Bin Stores Plans and Elevations 3141 111 Rev A

CONSULTEE NOTIFICATIONS & RESPONSES:-

- | | | |
|-----|---------------------------------------|------------------------------------|
| 1. | Environment Agency | No comments received |
| 2. | WSCC - Highways | No objection subject to conditions |
| 3. | Thames Water | No objection subject to conditions |
| 4. | CBC - Drainage Officer | No comments received |
| 5. | CBC - Property Division | Comments about parking |
| 6. | CBC - Planning Arboricultural Officer | No objection |
| 7. | Sussex Wildlife Trust | No comments received |
| 8. | CBC - Environmental Health | No objection subject to conditions |
| 9. | CBC - Refuse & Recycling Team | No objection |
| 10. | CBC - FP - Urban Design | No objection |
| 11. | Ecology Officer - Mike Bird | No objection |
| 12. | WSCC - Surface Water Drainage (SWD) | No objection |
| 13. | CBC - FP - Housing | |

SITE NOTICE & PRESS ADVERT:-

The Council has carried out its statutory publicity by placing an advert in the paper on the 24 February 2016 and displaying site notices on the 4 March 2016.

RESPONSES RECEIVED:-

20 letters of representation have been received objecting to the application on the following grounds:

Character and appearance

- Out of character and does not relate sympathetically with its surrounding area

- Inappropriate to the location in terms of scale, height, massing, density and character
- Balance of properties needed in the community
- Over developed cramped flats
- Impact on local infrastructure and services (schools and doctors)
- Site would be better used for houses rather than flats
- Existing dwellings on site have character
- Too dominant

Highways and access

- Area already suffers from congestion
- Increased traffic congestion
- Highway safety
- Insufficient/under provision for car parking for flats
- Will result in overflow car parking on street
- Not enough visitor parking
- Visibility at “junction of Forge Road and North Road is particularly difficult”
- Trees would impede visibility
- Cars already speed in the area.
- One parking space per flat is not enough

Other

- Concern regarding noise from the flats and proposed access
- Loss of sunlight to existing dwellings
- Overlooking on to neighbouring properties
- Concerns about publicity
- Light pollution
- Air pollution (Vehicle fumes into garden of no.54 North Road)
- Not enough social housing
- Area is in a control parking zone
- Lighting and security
- Too dense number of people
- Discrepancy with the plans in relation to the position of 54 North Road
- CIL contributions
- Removal of existing tree/landscaping on site
- Unsuitable for diverse groups
- More landscaping required

Non planning related objections include:

- ‘Anti-social parking’
- Anti-social behaviour
- Personal security
- References made to other developments at Woodfield Development and Arun Close
- Residents of the development should not be allowed to sublet/assign their parking spaces
- Right to buy
- Japanese knotweed on the site

REASONS FOR REPORTING TO COMMITTEE:-

1. The number of written representations received exceeding four with a recommendation to permit.
2. The number of dwellings proposed (small scale major 10-99 dwellings).

THE APPLICATION SITE:-

- 1.1 This application relates to Nos. 56A, 58 and 60 North Road located at the junction of Forge Road and North Road in Three Bridges. The site contains 3no detached bungalows fronting North Road with 30-40m rear gardens. The properties have splayed frontages set back on average of over 10

metres from the adjacent highway. No. 58 & 60 are in a staggered position in the street with no.56A set behind the building line of no.58. The dwellings are currently vacant with the northern and eastern boundaries of the site defined with construction hoarding.

- 1.2 The surrounding area is characterised by a mix of dwelling types. These include; detached bungalows; terraced, semi-detached and detached two-storey dwellings; a three-storey flatted development opposite Forge Road frontage known as Goepel Court; a three-storey town house development at nos.77-83A North Road and a two storey development to the south east at Brownjohn Court.

THE PROPOSED DEVELOPMENT:-

- 2.1 The application seeks permission for the demolition of 3no dwellings at nos. 56A, 58 and 60 North Road and erection of a residential development of 24 flats; comprising two blocks to provide 14 x two bedroom and 10 x one bedroom units.
- 2.2 The main block contains plots 1-20 and would wrap around the junction at North Road and Forge Road in an inverted U-shape layout. This building would be set back 8 metres from the public highway to reflect the existing building lines with the bungalows in North Road to the south and Goepel Court to the North. The second block comprising plots 20-24 would be positioned in the southern portion of the site in a T-shape layout creating a natural enclosure to the communal gardens to the east.
- 2.3 The main block fronting Forge Road and North Road would comprise a two storey element adjacent to no.56 North Road with a ridge height of 9.8metres stepping up to 2½ storeys with a maximum height of 12metres. The second block to the rear would be two storey and have a ridge height of 9metres. The materials and finishes to be used would include facing brick, render, vertical tile/weatherboard and roof tiles.
- 2.4 There would be one main vehicular access to the development from Forge Road for the car park to the south and west of the site. A total of 27 parking spaces would be provided to the rear of the site with two of the parking spaces fronting North Road.
- 2.5 A 5.5 metre high bin and bicycle store would be sited to the southern corner of the site with a landscaping/communal amenity space shown to the frontages and around the buildings to the rear including grassed areas and shrubs. Two further bin stores are would be provided in the layout.
- 2.6 During the course of the application amended plans have been received with the roof design altered to comprise full hipped roofs as opposed to the gable end and half hipped roofs originally proposed.

PLANNING HISTORY:-

- 3.1 CR/2009/0131/FUL – DEMOLITION OF 3 DWELLINGS AND THE ERECTION OF A RESIDENTIAL DEVELOPMENT COMPRISING 24 FLATS AND 5 DWELLINGS AT NOS 56A - 60 NORTH ROAD, THREE BRIDGES, CRAWLEY, WEST SUSSEX – REFUSED. The reasons for refusal related to the layout and density of development; its density, design, bulk, scale and siting; impact on the amenity of the neighbouring occupiers and future occupiers of the development and that no section 106 agreement was in place. The application was eventually dismissed at appeal by the Planning Inspector at that time.
- 3.2 CR/2005/0254/OUT – DEMOLITION OF EXISTING HOUSES AND ERECTION OF 16 X 1 BED, 4 X 2 BED, 1 X 3 BED APARTMENTS, 2 X 3 BED SEMI-DETACHED HOUSES, 3 X 2 BED TERRACED HOUSES WITH ASSOCIATED ACCESS AND PARKING AT NOS 56-60 NORTH ROAD, THREE BRIDGES, CRAWLEY– PERMIT. Subject to conditions, and restriction of the development to 2 ½ storeys in height. 26 units granted in total. The proposal was to be set back from the road by some 8m. 34 parking spaces were provided in the layout.

- 3.3 CR/2004/0040/FUL – DEMOLITION OF EXISTING PROPERTIES AND ERECTION OF 23 X 2 BED FLATS AND 9 X ONE BED FLATS WITH ASSOCIATED ACCESS AND PARKING AT NOS 56-60 NORTH ROAD, THREE BRIDGES, CRAWLEY– REFUSED. The reasons for refusal related to; the dominance of the main apartment block in the street scene; the proposal's impact on the amenities of the adjoining occupiers by means of loss of sunlight, privacy, outlook, noise and disturbance; the inadequate amount of outdoor amenity space; and the lack of provision for affordable housing.

PLANNING POLICY:-

National Planning Policy Framework (2012):

- 4.1 The National Planning Policy Framework (NPPF) was adopted in March 2012. The core principles introduced the presumption in favour of sustainable development in approving developments that accord with the development plan without delay or where the development plan is absent, silent or relevant policies are out of date, unless there would be significant adverse impacts or it would be contrary to the policies in the NPPF.
- 4.2 Paragraph 34 & 37 (Promoting sustainable transport). Plans and decisions should ensure developments that generate significant movement are located where the need to travel will be minimised and the use of sustainable transport modes can be maximised. Planning policies should aim for a balance of land uses within their area so that people can be encouraged to minimise journey lengths for employment, shopping, leisure, education and other activities.
- 4.3 Paragraph 47 & 49 (Delivering a wide choice of high quality homes) applies specifically to delivering homes and seeks to significantly boost the supply of housing. Housing applications should be considered in the context of the presumption in favour of sustainable development and are to deliver a wide choice of high quality homes, widen opportunities for home ownership and create sustainable, inclusive and mixed communities.
- 4.4 Paragraph 56 (Requiring good design). The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.

Crawley Borough Local Plan (2015-2030)

- 4.5 Policy SD1 (Presumption in Favour of Sustainable Development) states development will be supported where it complements Crawley's character as a compact town within a countryside setting, developed on a neighbourhood principle and maximises the use of sustainable travel; provides a safe and secure environment for its residents and visitors; and accords with the policies and objectives set out in this Plan unless material considerations indicate otherwise.
- 4.6 Policy CH1 (Neighbourhood Principle) states the neighbourhood principle will be protected and enhanced. Development within the neighbourhoods should mainly be residential, in-keeping with the existing neighbourhood. Mixed use and higher density development may be compatible with the existing structure of the neighbourhood, particularly if it is situated in sustainable locations.
- 4.7 Policy CH2 (Principles of Good Urban Design) seeks new development proposals will be required to respond to and reinforce locally distinctive patterns of development and landscape character.
- 4.8 Policy CH3 (Normal Requirements of All Development) states all proposals for development will be required to make a positive contribution to the area; be of a high quality urban design, provide and retain a good standard of amenity for all nearby and future occupants of land and buildings; be able to meet its own operational requirements necessary for the safe and proper use of the site; retain existing individual or groups of trees; incorporate "Secure by Design" principles and demonstrate how the Building for Life 12 criteria would be delivered.

- 4.9 Policy CH4 (Comprehensive Development and Efficient Use of Land) states development proposals must use land efficiently and not unduly restrict the development potential of adjoining land, nor prejudice the proper planning and phasing of development over a wider area
- 4.10 Policy CH5 (Standards for All New Dwellings) (including conversions) states all new dwellings must create a safe, comfortable and sustainable living environment, capable of adapting to the changing needs of residents through the application of the Lifetime Homes Standards. The minimum size for each dwelling should be based on the National internal and local external space standards.
- 4.11 Policy CH6 (Tree Planting and Replacement Standards) states landscape proposals for residential development should contribute to the character and appearance of the town by including at least one new tree for each new dwelling, of an appropriate species and planted in an appropriate location. Where development proposals would result in the loss of trees, applicants must identify which trees are to be removed and replaced in order to mitigate for the visual impact resulting from the loss of the tree canopies.
- 4.12 Policy H1 (Housing Provision) the Council will positively consider proposals for the provision of housing to meet local housing needs.
- 4.13 Policy H3 (Future Housing Mix) states that all housing development should provide a mix of dwelling types and sizes to address the nature of local housing needs and market demand.
- 4.14 Policy H4 (Affordable and Low Cost Housing) states that 40% affordable housing will be required from all residential developments and 10% low cost housing on developments of 15% dwellings or more, unless evidence provided to show the site cannot support these requirements from a viability perspective.
- 4.15 Policy ENV2 (Biodiversity) states all development proposals will be expected to incorporate features to encourage biodiversity where appropriate, and where possible enhance existing features of nature conservation value within and around the development.
- 4.16 Policy ENV6 (Sustainable Design and Construction) all homes will be required to meet the strengthened on-site energy performance standards of Building Regulations, and any subsequent increased requirements. A Sustainability Statement demonstrating how sustainability objectives have been achieved during the design process, or will be achieved during the, and construction processes should be submitted.
- 4.17 Policy ENV8 (Development and Flood Risk) states development proposals must avoid areas which are exposed to an unacceptable risk from flooding, and must not increase the risk of flooding elsewhere.
- 4.18 Policy ENV9 (Tackling Water Stress) states new dwellings should where viable and technically feasible, meet the Building Regulations optional requirement for tighter water efficiency.
- 4.19 Policy IN1 (Infrastructure Provision). Development will be permitted where it is supported by the necessary infrastructure both on and off site and if mitigation can be provided to avoid any significant cumulative effects on the existing infrastructure services. The Council will seek to implement a Community Infrastructure Levy (CIL) through the relevant processes of which is yet to be adopted.
- 4.20 Policy IN3 (Development and Requirements for Sustainable Transport). Development should be concentrated in locations where sustainable travel patterns can be achieved through the use of the existing transport network, including public transport routes and the cycling and walking network. Developments should meet the access needs they generate and not cause an unacceptable impact in terms of increased traffic congestion or highway safety. Developments will be permitted unless the cumulative impact on the transport network is severe and cannot be satisfactorily mitigated.

- 4.21 Policy IN4 (Car and Cycle Parking Standards) states development will be permitted where the proposals provide the appropriate amount of car and cycle parking to meet its needs when it is assessed against the borough council's car and cycle parking standards.

Supplementary Planning Guidance and Documents

- 4.22 Relevant SPD's/SPG's are (SPG) Note 4 'Private Outdoor Space'; (SPG) Note 17 'Higher Density Housing' and The Planning Obligations and Section 106 Agreements Supplementary Planning Document 2008 which includes parking standards and details of required infrastructure mitigation.
- 4.23 Other SPD's include the emerging Draft Urban Design SPD; Draft Green Infrastructure SPD & Draft Planning & Climate Change SPD which are emerging SPG's currently subject to consultation.
- 4.24 Technical Housing Standards – Nationally Described space standards which outlines minimum standards of accommodation for development and private outdoor amenity space.

PLANNING CONSIDERATIONS:-

- 5.1 The main planning considerations in the determination of this application are:
- Principle of development
 - Design, appearance, layout & impact on the street scene
 - The acceptability of the proposed development for future occupiers
 - Impact on neighbouring properties
 - Parking and highway safety & whether it can meet its own operational needs
 - Affordable Housing
 - Impact on trees
 - Sustainability
 - Infrastructure Contributions
 - Other Matters

Principle of development

- 5.2 The use of the site for residential is consistent with NPPF objectives to promote and encourage residential development on appropriate sites. The site is located within the built-up area boundary of Crawley as defined by the local plan map and is within an existing sustainable residential area of Three Bridges. The principle of development in this area is considered acceptable and in accordance with policy SD1 of the Local Plan.

Design, appearance, layout & impact on the street scene

Design appearance & layout

- 5.3 The proposal has been modified to reduce its impact on the street scene by incorporating full hipped roofs rather than the original gables and half hipped roofs proposed which reduces the overall bulk of the proposal.
- 5.4 The main building alters in scale in response to the adjacent buildings. This allows a progression along the North Road street scene with a single story detached dwellings to the south. The first section of the building is scaled to two storeys (9.8metres high), relating more sympathetically with the height of the adjacent chalet bungalow at no. 54 North Road (6metres high); followed by a rise to 2½ storeys (12metres high) on the corner junction of North Road and Forge Road mirroring the height of Goepel Court to the north. To the west the building would continue to be 2½ storeys with a drop in height to 11metres with a full hipped roof and a separation distance of 29metres to the two storey semi-detached dwellings at no.22 and no.24 Forge Road. It is considered that staggering the height of the buildings as indicated on the plans would not out of keeping and would relate sympathetically with the surrounding dwellings.

- 5.5 The appearance of the proposal would reflect the local vernacular style using a variety of brick, render and tiles finishes that serve to break up the building facades. The use of flat roof dormers windows with Juliet balconies and a front gable bay would also create visual interest that is considered would not be out-of-keeping with the area.
- 5.6 The main building would be set back from the existing public highway by not less than 8 metres reflecting the existing building lines of North Road and Forge Road. The second building on the southern edge of the site would be two storey (9metres high, with a fully hipped roof) reflecting the materials and finishes of the main building. This building would create a natural enclosure to the communal garden areas behind the main block.
- 5.7 The main area of car parking for the buildings would be to the west, with a secondary access off North Road for two parking spaces. The main parking area would be overlooked by the units providing natural surveillance and security and views of the car park would be screened by landscaping.
- 5.8 In this regard, it is considered the design and appearance and layout is acceptable and would be sympathetic and respect the design, appearance and pattern of development in the area in accordance with the policies CH1, CH2 and CH3 of the Local Plan.

Impact on the street scene

- 5.9 The main building would be setback from the existing road frontages by not less than 8 metres, allowing for the growth of tree/landscaping in front of the main building which over time would mature and soft the appearance of the development. This building in terms of its design and siting address the street and building lines and it considered to respond positively to its surroundings.
- 5.10 Views of the second building would not be visible from North Road to the south and would be screened by the main building to the north as it would be set behind the main building of which is set higher.
- 5.11 The proposal would be viewed in context with a variety of dwelling types and roof forms. The use of fully hipped roofs and staggered heights as outlined above is considered acceptable and would relate sympathetically with the character of the surrounding street scene.
- 5.12 Therefore it is not considered the proposal would cause any harmful impact to the visual amenity of the street scene in accordance with the policies CH1, CH2 and CH3 of the local plan.

The acceptability of the proposed development for future occupiers

Space standards & Layout

- 5.13 The proposal would comprise 24 flats; 14 x two bedroom and 10 x one bedroom units equating, to a 3 person bed space and a 2 person bed space units with an occupancy level of 62 people.
- 5.14 The National Standards sets out the gross internal floor area (GIA) of a two bedroom (3 person bed space) over a single storey is 50sqm and a one bedroom (2 person bed space) over a single storey is 61sqm. The units have been assessed against these space standards and all of the units proposed would comply with these floor areas with the smallest unit providing a floor area of 50.5sqm and the largest unit providing 68.4sqm. All of the units would have adequate storage space as outlined in these standards.
- 5.15 Each unit would be designed with an open plan layout with adjoining kitchen and dining areas. The main habitable rooms would have dual aspect and take up the advantage of natural light and views which is considered acceptable. The habitable areas would also face the communal amenity area with the bed room areas located to the front facing Forge and North Road apart from unit 14.

- 5.16 The internal separation distances from the first floor habitable rooms on the eastern elevation of the west wing to the western elevation of the east wing would be 20 metres. Normally 21 metres would need to be provided and given this proposal would relate to a new development, future occupiers would be aware of this. Therefore on balance it is not considered the rooms facing opposite would cause any harmful direct overlooking into neighbouring units and it is not considered this 1 metre deficiency would be a sufficient reason to refuse the application given it would be acceptable in all other aspects.

Amenity Area

- 5.17 A mix of individual and communal amenity space would be provided throughout the site. The indicative layout of the ground floor units would have direct access to private amenity areas of which would be enclosed by a low hedge providing a secure and safe area. The remaining areas would be communal with amenity areas to the front and rear of the site.
- 5.18 These areas would be south facing providing daylight and sunlight and be overlooked by the units providing natural surveillance and it is considered the amenity areas provided would create a usable, private outdoor amenity space for the future occupiers in accordance with the principles of SPG4.
- 5.19 It is therefore considered the proposed internal layouts would accord with the Nationally Described space standards by providing adequate living space for future occupiers and provide an amenity area in accordance with the principles of Supplementary Planning Guidance note 4.

Impact on neighbouring properties

- 5.20 The main building would front North Road to the east and Forge Road to the north. SPG Note 4 states a 21 metre separation distance should normally be provided between opposite first floor habitable windows. The separation distance of the main building would be no less than 23 metres from the first floor windows on the proposed eastern elevation to the western gable wall of no.26a Forge Road. The separation distance from the first floor windows on the proposed northern elevation would be no less than 26 metres from the southern elevation of Goepel Court and the separation distance from the first floor windows on the proposed western elevation to the eastern elevation of no.22-24 Forge Road would be no less than 29 metres.
- 5.21 The second building to the south would have a separation distance of 23 metres from the first floor windows on the proposed eastern elevation of this building to the western elevation/ three dormer windows of no.56 North Road. In addition the ground floor and first floor windows on the southern elevation of this building would be of obscured glazing mitigating any direct overlooking to no.54 North Road to the immediate south.
- 5.22 Given these separation distances it is not considered the development as proposed would cause any harmful impact on neighbouring amenity in terms of direct overlooking or loss of privacy on these properties. It is also considered that the proposal would not result in an unacceptable loss of light, loss of outlook to adjoining occupiers due to the orientation of the proposed buildings to the surrounding neighbouring occupiers.
- 5.23 It is therefore considered the proposal would accord with the policy CH3 of the local plan and with Supplementary Planning Guidance note 4.

Parking and highway safety & whether it can meet its own operational needs

- 5.24 The site falls within the Three Bridges parking zone, as set out in Crawley Borough Council's Planning Obligations and S106 Agreements SPD. The parking standards are maximum standards, not minimum, and at the standards would require a 1 bedroom unit to have 1 car parking space and a 2 bedroom unit to have 1.2-1.5 car parking spaces equating to 10 spaces for the 1 bedroom units and 17-21 spaces for the 2 bedroom units.

- 5.25 Given these are maximum standards the spaces required would be between 27-31 spaces to be provided as outlined in the SPG. The proposed development would provide a total of 27 parking spaces (1 space per unit and 3 visitor spaces). Therefore it is considered the level of parking provision on the site is acceptable in this location and in line with the parking numbers.
- 5.26 The majority of the spaces would be sited to the rear area with the main vehicle access onto Forge Road to the north with a new access and two off street car parking spaces with a turning area provided along North Road. The car parking area would incorporate a turning area for refuse and emergency vehicles which is considered acceptable. The plans also show a cycle store to provide 24 bicycles, a shortfall of 14 cycle spaces plus 3 visitor as outlined in the SPG. The applicant has agreed to provide a further 17 cycle spaces which has been provided through an amended plan.
- 5.27 West Sussex Highways have been consulted and have raised no objection to the proposal from a highway perspective subject to standard conditions relating to visibility commenting the *“The level of traffic generated by the site anticipated to be somewhere between 5 and 14 peak hour trips which is unlikely to have a significant impact on the local road network”*.
- 5.28 The site is sustainably located in proximity to shops and services with several high frequency bus routes within close walking distance. Three Bridges train station is approximately 600m south-east. The location of the site would promote and encourage the use of sustainable transport in accordance with policy IN3 of the Crawley Borough Plan 2015-2030.
- 5.29 In conclusion subject to the provision of cycle parking, the overall level of parking provision would meet the adopted parking standards, operationally the parking arrangement is considered acceptable and there are no negative highway safety impacts. Therefore the proposed development would accord with policy CH3 of the Crawley Borough Plan 2015-2030 and the objectives of the NPPF.

Affordable Housing and Housing Mix

- 5.30 The development would provide a mix of unit type and sizes and would make a contribution towards the number of one and two bedroom units needed as identified in the Council's Strategic Housing Market Assessment.
- 5.31 The applicant as a registered affordable housing provider, Places for People have always intended this site for affordable housing, but in the current market they are unable to provide rental tenure without including some degree of cross-subsidy from market sale units.
- 5.32 As a result, the options that were considered were to deliver the scheme as 100% affordable housing with all 24 units offered as shared-ownership, or alternatively to offer 40% affordable housing on a 70/30 tenure split resulting in 7 units for affordable rent and 3 units for shared-ownership.
- 5.33 The Head of Strategic Housing and Planning Services considered that an all shared-ownership scheme would be preferred under the circumstances as it addresses a specific housing need while retaining the whole site for affordable housing. In planning policy terms this can be justified as achieving an additional 14 windfall units for affordable housing, which would otherwise have been lost to the open market.
- 5.34 The marketing of these units will be done through the Help-to-Buy agent in the first instance and will target applicants either living or working in Crawley who have registered an interest in acquiring a shared-ownership property in Crawley and will further prioritise those applicants that may free-up existing affordable housing stock.
- 5.35 To safeguard the perpetuity of affordable housing on this site within the parameters of planning policy H4 of the local plan, the section 106 legal agreement will offer both options on an either/or basis, either as 100% shared-ownership or as 40% affordable housing on a 70/30 tenure split.

Impact on trees

- 5.36 The application site has one protected tree on site fronting North Road however on inspection this tree was absent and from the aerial photos appears to have been removed as far back 1980. There are other trees off site covered by preservation orders to the southern boundary with no.48 Three Bridges Road. The application has been accompanied by a tree survey and Arboricultural report.
- 5.37 The Councils Arboricultural Officer has been consulted and has raised no objection to the development provided the development is carried out as outlined in the details submitted in this report. Therefore it is considered that the layout and level of development can be accommodated without harm to these important trees, which are important to the visual amenity of the surrounding area.
- 5.38 The layout plan has indicated that twenty new trees would be planted throughout the site. This is a shortfall of four trees as required under policy CH6 of the Local Plan. The applicant has confirmed four additional trees would be provided on the site which would be secured by way of a landscaping condition.

Sustainability

- 5.39 The design and access statement indicates the development would help reduce CO2 emissions by incorporating sustainable measures such as high levels of insulation, low energy/ high efficiency appliances, high performance double glazed windows, control of water through efficient controls and drainage and air tightness to reduce heat loss and subject to these measures receiving Building Regulations approval the development would be in accordance with policy ENV6 of the local plan.

Infrastructure Contributions

- 5.40 West Sussex County Council have advised that the following contributions would be sought for local infrastructure and services. Contributions have been sought by way of a section 106 legal agreement of which are distributed below.

S106 type	Monies Due
Education - Primary	£4,384
Education - Secondary	£4,718
Education - 6th Form	£1,105
Libraries	£3,409
Waste	No contribution required
Fire & Rescue	£3,064
No. of Hydrants	To be conditioned if required
TAD	£29,865
Total Contribution	£46,545

Other Matters

Flooding

- 5.41 The site is not located within an area of flood risk as outlined on the Environment Agency's flood map however measures have been sought by West Sussex Flood Risk Management Team in relation to Sustainable Urban Drainage systems of which would be controlled by way of condition.

Noise

- 5.42 Any noise generated from the proposal would be during the construction period only. The site is located within a built-up residential area and the application relates to an additional residential use. It is therefore not considered there would be any cumulative impact with regards to noise as a

result. A condition relating to hours construction has been requested by the Environmental Health Officer. This is controlled by other Environment Health legislation and can be covered by an informative.

Ecology

- 5.43 The Ecology Advisor has inspected the submitted ecology report and has indicated no objection to the proposal on ecology grounds.

Landscaping

- 5.44 The landscaping plan provided is an indicative illustration. Further details indicating hard and soft landscaping and trees would be secured by way of condition.

Publicity

- 5.45 The Council has carried out its statutory publicity by placing an advert in the paper on the 24 February 2016 and displaying site notices on the 4 March 2016.

CONCLUSIONS:-

- 6.1 In conclusion it is considered the residential redevelopment is acceptable in this location and the bulk, scale, siting and design of the proposal is considered to be in keeping with the site and the surrounding area and would not have a harmful impact to the visual amenity of the existing street scene.
- 6.2 The proposal would make provision for sufficient car parking, cycle and refuse storage, outdoor open space and would conform with the separation distances to neighbouring properties and standards as outlined in the Supplementary Planning Guidance notes and the Nationally Described space standards and is not considered the proposed development would have a harmful impact on residential amenity of neighbouring properties or to future occupiers of the development.
- 6.3 Therefore the proposal is in accordance with the policies outlined in the Crawley Borough Local Plan 2015-2030 and Supplementary Planning Guidance notes and it is recommended to grant permission for this application subject to conditions and subject to the conclusion of a S106 Agreement to secure the affordable housing and infrastructure contributions.

RECOMMENDATION RE: CR/2016/0166/FUL

PERMIT subject to the following condition(s) and the conclusion of a section 106 legal agreement to secure the affordable housing and infrastructure contributions:

1. The development hereby permitted shall be begun before the expiration of 3 years from the date of this permission.
REASON: To comply with Section 91 of the Town & Country Planning Act 1990.
2. The development hereby permitted shall not be carried out other than in accordance with the approved plans as listed in this Decision Notice save as varied by the conditions hereafter.
REASON: For the avoidance of doubt and in the interests of proper planning.

Materials/Details

3. No development shall be carried out (with the exception of demolition) unless and until a schedule of materials and finishes and, where so required by the Local Planning Authority, samples of such materials and finishes to be used for external walls (and roofs) of the proposed building(s) have been submitted to and approved by the Local Planning Authority.
REASON: To enable the Local Planning Authority to control the development in detail in the interests of amenity by endeavouring to achieve a building of visual quality in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

4. Prior to commencement of any works on site, detailed plans and particulars of the land levels and the finished floor levels of the buildings shall be submitted to and approved in writing by the Local Planning Authority, and the building shall be constructed in accordance with the approved levels.
REASON: To enable the Local Planning Authority to control the development in detail in the interests of amenity in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

Walls/Fencing

5. The development shall not be occupied until details of screen walls and/or fences have been submitted to and approved by the Local Planning Authority and no dwellings/buildings shall be occupied until such screen walls and/or fences associated with them have been erected.
REASON: In the interests of amenity in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

Landscaping

6. The development shall not be occupied until a hard and soft landscaping scheme has been submitted to, and approved by, the Local Planning Authority. The approved details of the landscaping shall be carried out in the first planting and seeding season, following the occupation of the building or the completion of the development, whichever is the sooner, and any trees or plants which, within a period of five years from the completion of development die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with other of similar size and species, unless the Local Planning Authority gives written consent to any variation.
REASON: In the interests of the amenity and of the environment of the development in accordance with Policy CH3 & CH6 of the Crawley Borough Local Plan 2015-2030.

Trees

7. Prior to commencement of any works on site the tree protection measures shall be implemented and retained throughout the construction period in accordance with the approved details detailed within the Arboricultural Implications Assessment and Preliminary Method Statement Survey by Stephen Milligan dated 3 September 2015.
REASON: To ensure the retention and maintenance of trees and vegetation which are an important feature of the area in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

Highways

8. No development shall be carried out on the site, including any works of demolition, until a Construction Management Plan (CMP) has been submitted to and approved in writing by the Local Planning Authority. The approved plan shall thereafter be implemented and adhered to throughout the entire construction period. The plan shall show the site set up during construction and provide details as appropriate but not necessarily be restricted to the following matters:
- The anticipated number, frequency and types of vehicles used during construction,
 - The areas of where all temporary contractors' buildings are to be sited,
 - The method of access and routing of vehicles during construction,
 - The parking of vehicles by site operatives and visitors,
 - The storage of plant and stacks of materials used in construction of the development,
 - The loading and unloading areas of plant, materials and waste,
 - Details of any temporary lighting for construction and security,
 - Measures to control the emission of dust and dirt during demolition and construction, to include an effective water source to either manually or mechanically "damp down" dust and related debris generated at the site.
 - The provision of wheel washing facilities and other works required to mitigate the impact of construction upon the public highway (including the provision of temporary Traffic Regulation Orders),
 - The erection and maintenance of security hoarding.

REASON: To avoid undue congestion of the site and consequent obstruction to access in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

9. The development shall not be occupied until such time as the vehicular access has been constructed in accordance with plans and details submitted to and approved in writing by the Local Planning Authority.

REASON: In the interests of road safety in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

10. The development shall not be occupied until visibility splays of 2.4metres by 43 metres have been provided at the proposed site vehicular access onto Forge Road in accordance with plans and details submitted to and approved in writing by the Local Planning Authority. Once provided the splays shall thereafter be maintained and kept free of all obstructions over a height of 0.6 metre above adjoining carriageway level or as otherwise agreed.

REASON: In the interests of road safety in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

11. The development shall not be occupied until the parking spaces and turning facilities shown on the approved plans have been provided and constructed. The areas of land so provided shall not thereafter be used for any purpose other than the parking and turning of vehicles.

REASON: In the interest of road safety and to ensure that adequate and satisfactory provision is made for the accommodation of vehicles clear of the highways in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

12. The development shall not be occupied until such time as the existing vehicular access points onto North Road have been physically closed in accordance with plans and details submitted to and approved in writing by the Local Planning Authority.

REASON: In the interests of road safety in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

Sustainable Measures

13. No development shall be carried out (with the exception of demolition) until finalised detailed surface water drainage designs for the site, based on sustainable drainage principles, for the development have been submitted to and approved in writing by the Local Planning Authority. The drainage designs should demonstrate that the surface water runoff generated up to and including the 100 year, plus 30% for climate change, critical storm will not exceed the run-off from the current site following the corresponding rainfall event.

REASON: To safeguard the area against the risk of flooding in accordance with Policy ENV8 of the Crawley Borough Local Plan 2015-2030.

14. No development shall be carried out (with the exception of demolition) until full details of the maintenance and management of the SUDs system is set out in a site-specific maintenance manual and submitted to, and approved in writing, by the Local Planning Authority. The scheme shall subsequently be implemented in accordance with the approved designs.

REASON: To safeguard the area against the risk of flooding in accordance with Policy ENV8 of the Crawley Borough Local Plan 2015-2030.

15. The development shall not be occupied until details have been submitted to the Local Planning Authority to demonstrate that each unit shall achieve a water efficiency standard by consuming not more than 110 litres per person per day maximum water consumption.

REASON: In the interests of sustainable design and efficient use of water resources in accordance with emerging policy ENV9 of the Crawley Borough Local Plan 2015-2030

Cycle/Bin

16. The cycle storage areas shall be implemented prior to the occupation of the flats in accordance with the approved details and shall thereafter be retained for this use.

REASON: To provide alternative travel options to the use of the car in accordance with Policy CH3 & IN4 of the Crawley Borough Local Plan 2015-2030.

17. The bin storage areas shall be implemented prior to the occupation of the flats in accordance with the approved details and shall thereafter be retained for this use.
REASON: To meet its operational requirements in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

Ecology

18. The development shall be carried out in accordance with the recommended mitigation measures set out in the Ecology Report by Wild Frontier Ecology, revised November 2015 and details of the actual habitat enhancement measures to be implemented, based on suggestions in this report, shall be submitted to, and approved in writing, by the local planning authority. The approved details shall be implemented in full.
REASON: To ensure that the proposals avoid adverse impacts on protected and priority species and contribute to a net gain in biodiversity, in accordance with Policy ENV2 of the Crawley Borough Local Plan 2015-2030.

Obscured glass

19. The windows on southern elevation A-A of plots nos. 20-24 adjacent shall at all times be glazed with obscured glass and apart from any top-hung vent, be fixed to be permanently non-opening.
REASON: To protect the amenities and privacy of the adjoining property, in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

Other

20. Provision shall be made for communal aerial facilities to serve all residential units in accordance with details to be submitted to and agreed in writing by the Local Planning Authority.
REASON: In the interests of the visual amenities in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

INFORMATIVES

Environmental Health

1. Within the boundaries of Crawley Borough Council the Control of Pollution Act 1974 is used to control noise from construction sites. Section 60 of the Act permits Local Authorities to specify the hours the noisy works are permitted as follows:
0800 to 1800 Monday to Friday and
0800 to 1300 on Saturday.
No noisy construction works taking place on Sundays, Bank Holidays, Public Holidays, Christmas Day, Boxing Day or New Years' Day. No deliveries and/or collections will take place outside of these times.
2. The developer shall employ at all times the best practical means to minimise noise disturbance to nearby residents. Noise associated with the site shall be managed using best practicable means (BPM) All plant and machinery shall be properly silenced and maintained in accordance with the manufacturer's instructions and noise managed in accordance with the requirements of BS5228. 'Code of practice for noise and vibration control on construction and open sites'.

Highways

1. The applicant is advised to contact the Community Highways Officer covering the respective area (01243642105) to obtain formal approval from the highway authority to carry out the site access works on the public highway.

Waste Comments

1. Surface Water Drainage - With regard to surface water drainage it is the responsibility of a developer to make proper provision for drainage to ground, water courses or a suitable sewer. In respect of surface water it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of groundwater. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0800 009 3921. Reason - to ensure that the surface water discharge from the site shall not be detrimental to the existing sewerage system.
2. There are public sewers crossing or close to your development. In order to protect public sewers and to ensure that Thames Water can gain access to those sewers for future repair and maintenance, approval should be sought from Thames Water where the erection of a building or an extension to a building or underpinning work would be over the line of, or would come within 3 metres of, a public sewer. Thames Water will usually refuse such approval in respect of the construction of new buildings, but approval may be granted in some cases for extensions to existing buildings. The applicant is advised to contact Thames Water Developer Services on 0800 009 3921 to discuss the options available at this site.
3. A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 02035779483 or by emailing wwriskmanagement@thameswater.co.uk. Application forms should be completed on line via www.thameswater.co.uk/wastewaterquality

Water Comments

1. With regard to water supply, this comes within the area covered by the Southern Water PLC. For your information the address to write to is - Southern House, Yeoman Road, Worthing, West Sussex BN13 3NX Tel - (0845) 2780845

Water efficiency

1. The water efficiency standard required under condition 16 is the 'optional requirement' detailed in Building Regulations Part G Approved Document (AD) Building Regulations (2015), at Appendix A paragraph A1

NPPF Statement

1. In determining this planning application, the Local Planning Authority assessed the proposal against all material considerations and has worked with the applicant in a positive and proactive manner based on seeking solutions where possible and required, by:
 - Providing advice in a timely and manner through pre-application discussions/correspondence.
 - Liaising with consultees and the agent and discussing the proposal where considered appropriate and necessary in a timely manner during the course of the determination of the application.
 - Seeking amended plans and additional information to address identified issues during the course of the application.

This decision has been taken in accordance with the requirement in the National Planning Policy Framework, as set out in article 35, of the Town and Country Planning (Development Management Procedure) Order 2015.



Crawley Borough Council,
Town Hall,
The Boulevard,
Crawley,
West Sussex RH10 1UZ

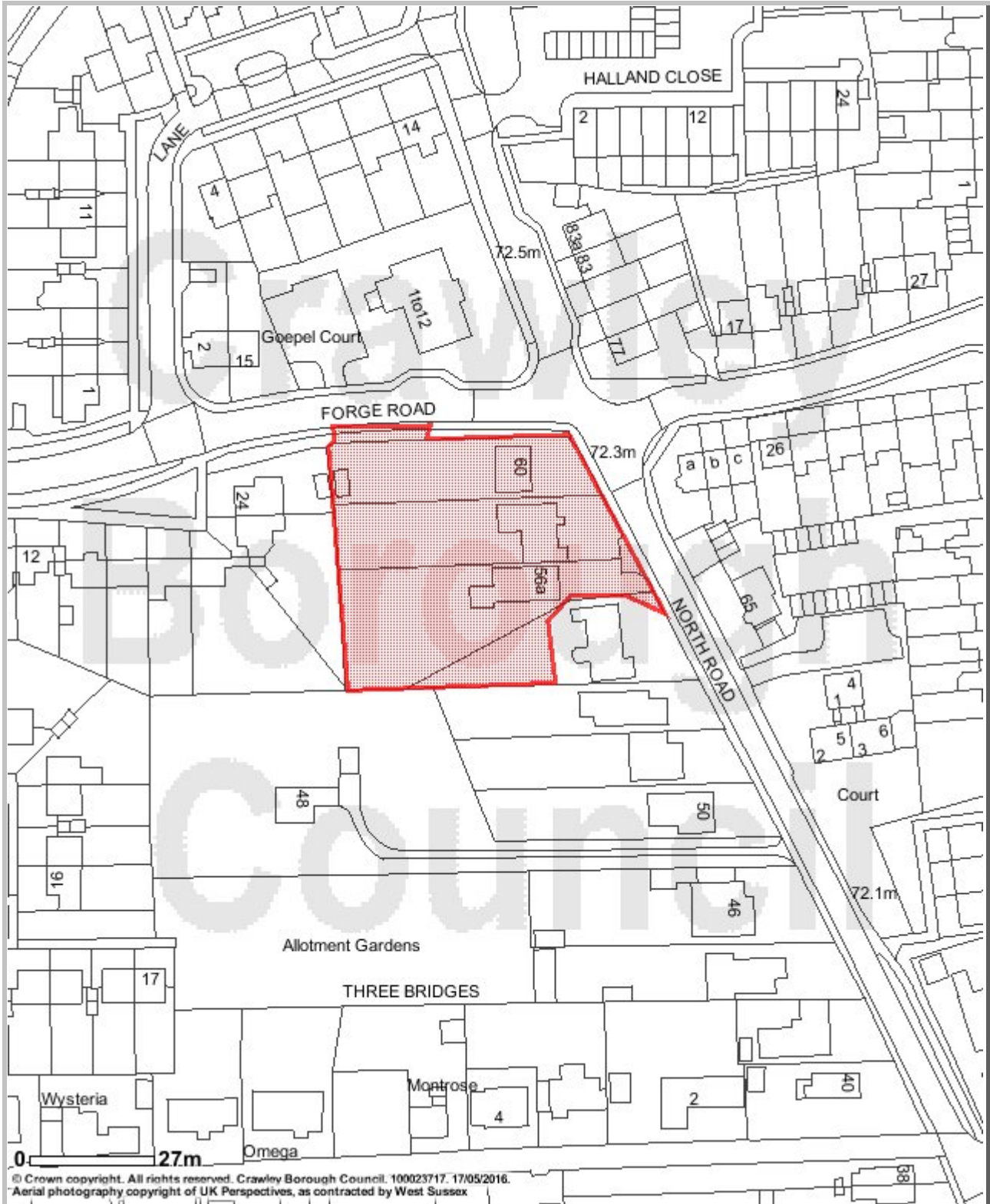
Tel: 01293 438000
Fax: 01293 438603

CR/2016/0166/FUL

Date 4 May 2016

Approx. Scale 1:1,250

**56 - 60 NORTH ROAD, THREE BRIDGES,
CRAWLEY**



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REFERENCE NO: CR/2016/0193/FUL

LOCATION: [40 CRABBET ROAD, THREE BRIDGES, CRAWLEY, RH10 1NE](#)
PROPOSAL: ERECTION OF FRONT & REAR EXTENSIONS AND LOFT CONVERSION (AMENDED PLANS RECEIVED)

TARGET DECISION DATE: 27 May 2016

CASE OFFICER: Miss D. Angelopoulou

APPLICANTS NAME: Mr Ahmer Shafiq

AGENTS NAME: RDjW Architects Limited

PLANS & DRAWINGS CONSIDERED:

No 4837-001 Amended_Existing Site Plan, No 4837-002 Existing Ground Floor, No 4837-003 Existing First Floor, No 4837-004 Existing Elevations, No 4837-005 RevA Amended_Proposed Site Plan, No 4837-006 RevA Amended_Proposed Ground Floor, No 4837-007 Proposed First Floor, No 4837-008 Proposed Second Floor, No 4837-009 RevB Amended_Proposed Elevations, No 4837-010 RevB Proposed Site Plan with Parking

CONSULTEE NOTIFICATIONS & RESPONSES:-

None

NEIGHBOUR NOTIFICATIONS:-

137, 139 and 141 The Birches, Three Bridges; 17, 19, 21, 32, 38, and 42 Crabbet Road, Three Bridges.

RESPONSES RECEIVED:-

Eight representations have been received from nearby occupiers raising objections to the proposal on the following grounds:

- Scale of the intended works – front extension would be inappropriate and would cause loss of light and privacy and maybe there would be a need for party wall agreement.
- Design and appearance of the front extension and impact on the street scene and not in keeping with the other properties in the road.
- Impact on car parking arrangements.
- The proposed dormer and loft conversion would cause overlooking and loss of privacy.
- Access to the shared driveway and rear garage would be restricted.

Re-consultation of the above-mentioned neighbours regarding the amended plan: 22/04/2016 for 14 days with expiry date: 06/05/2016.

One representation of objection has been received stating that since the proposed single storey rear extension has not been amended, the garage to the rear would be restricted and would not be in keeping with the other properties in the road.

REASON FOR REPORTING TO COMMITTEE:-

The number of objections received to a recommendation to permit.

THE APPLICATION SITE:-

- 1.1 The application site relates to a two storey semi-detached dwelling on the north side of Crabbet Road within the neighbourhood of Three Bridges. The property itself is constructed of brick and render with a tiled roof and has flat roof canopy above the existing front porch. The boundary to the rear and north-western side is marked by a 1.8m high fencing, whilst to the north-eastern side there was open boundary. However, during a second site inspection on 23/05/2016, No.42 Crabbet Road has erected a 1.8m high fence along part of the boundary between No 40 and 42. There is a shared driveway between Nos 40 and 42 leading to the rear of these houses. No 42 has a garage in the rear. Currently the dwelling does not benefit from any hard surfaced off road parking spaces as the front garden is laid to grass. It is understood that there was a garage to the rear, but it has been demolished at some point.

THE PROPOSED DEVELOPMENT:-

- 2.1 Planning permission is sought for the erection of single storey front and rear extensions and loft conversion. The proposed single storey front extension, which has been amended since originally submitted, would project approximately 1.4 metres beyond the main house frontage to the south-east and would measure approximately 2.5 metres in width and 3.3 metres to the ridge. Its front elevation would contain an entrance door and its south-eastern side elevation would contain a window. Internally, it would provide an enlarged hallway and WC.
- 2.2 The proposed single storey rear extension would project approximately 3.3 metres from the rear elevation and would extend the full width of the property. It would be set away from the rear site boundary by approximately 12 metres. It would have a mono-pitch roof and its rear elevation would contain two doors and one window. It would rise to a height of 3.4 metres to the ridge and would be 2.5 metres at the eaves. Internally, it would provide an enlarged kitchen and living area.
- 2.3 The proposed loft conversion would involve the construction of flat roofed rear dormer which as amended would be below the existing ridge and would measure 5.5 metres in width. It would also involve the insertion of two roof lights flush on the front roof elevation with no increase in height of the existing roof ridge. It would be constructed in tile hanging to match the existing roof and would incorporate one master bedroom and en-suite bathroom.

PLANNING HISTORY:-

- 3.1 There is no recent relevant planning history.

PLANNING POLICY:-

National Planning Policy Framework (2012) (NPPF)

- 4.1 The NPPF states that the applications for planning permission must be determined in accordance with the development plan unless material considerations indicate otherwise. More specifically:
- Paragraph 14 – Presumption in favour of sustainable development. At the heart of the NPPF is a presumption in favour of sustainable development, which should be seen as a golden thread running through both plan-making and decision-taking.
 - Paragraph 17 – Core planning principles. Always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings.
 - Section 7 – Requiring good design. The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.

Crawley Borough Local Plan (2015-2030) (adopted December 2015)

4.2 The relevant policies include:

- Policy SD1: Presumption in Favour of Sustainable Development. In line with the planned approach to Crawley as a new town, and the spatial patterns relating to the neighbourhood principles, when considering development proposals the council will take a positive approach to approving development which is sustainable.
- Policy CH2: Principles of Good Urban Design in order to assist in the creation, retention or enhancement of successful places.
- Policy CH3: Normal Requirements of All New Development states all proposals for development in Crawley will be required to make a positive contribution to the area, be of a high quality design, provide and retain a good standard of amenity for all nearby and future occupants of land and buildings, and be able to meet its own operational requirements necessary for the safe and proper use of the site. Development proposals must adhere to any relevant supplementary planning guidance produced by the council including residential extensions.

Supplementary Planning Guidance Note: 5 – Residential Extensions (2001):

4.3 Supplementary Planning Guidance Note 5 ‘Residential Extensions’ is a non-statutory document which supplements the policies of the Local Plan and is applicable to this application. It contains guidelines on the standards the Council expects for the design of extensions and improvements. In particular, it states that:

- *‘Only in exceptional circumstances will extensions which project more than 1.5m beyond the main house frontage be permitted’.*
- *‘Single storey extension at the rear of semi-detached properties which project more than 3.3m is not usually acceptable’.*
- *‘Dormers should not dominate the roof in which they are set: Keep dormers to the rear. As a general rule dormers should be no larger than is necessary to contain windows of the same size as those on existing facades. Dormers should be placed so that they are lower than the highest part of the roof in which they are set. They should be set back from the facade of the house so that a proportion of the existing roof remains below the dormer. Where possible the materials and detailing of dormers should match those elsewhere on the main roof.’*

Supplementary Planning Document 2008 – Planning Obligations and S106 Agreements:

4.4 The Annex B of this Supplementary Planning Document is also relevant and contains guidelines on the adopted parking standards, such as the maximum parking standards for a 3 plus bedroom dwelling in this location are 2-3 spaces.

PLANNING CONSIDERATIONS:-

5.1 The main planning issues in the determination of this application are:

- The design & appearance of the proposal and its impact on the dwelling, street scene & wider area
- The impact on neighbouring properties and amenities
- Parking considerations

The design & appearance of the proposal and its impact on the dwelling, street scene & wider area

5.2 The proposed single storey front extension would project 1.4 metres beyond the main house frontage. During the course of the determination of the application amended plans have been received reducing the width of the front extension as there were concerns that a front extension across the full width of the property would have resulted in a detrimental impact on neighbouring amenities and the street scene. The street scene of Crabbet Road is characterised by various front extensions, the design of the proposed front extension as amended is considered to be relatively modest and if finished in materials to match would sympathetically relate to the existing dwelling.

The proposed depth is in accordance with the Supplementary Planning Guidance Note 5 – Residential Extensions and is considered a relatively minor addition to the property.

- 5.3 The proposed single storey rear extension would project 3.3 metres from the rear elevation and at this depth complies with the design guidance contained in the SPG 5. The proposed rear extension as designed is not considered to dominate the existing dwelling as it would still appear as a subservient element to the host dwelling and would blend well by virtue of matching external materials. In addition, given that it would retain a length of approximately 12 metres of rear garden, it is also considered that more than adequate outdoor rear amenity space would remain.
- 5.4 The rear dormer as amended would be below the existing roof ridge and a proportion of the existing roof would remain below the dormer, and would be constructed in tile hanging to match the existing roof in accordance with the SPG 5. Whilst the proposed rear dormer is considered to be large, it meets the criteria which are outlined in Schedule 2, Part 1, Class B ‘additions etc to the roof of a dwellinghouse’ of the Town and Country Planning (General Permitted Development) (England) Order 2015 and so in itself would not need planning permission. The window positioning and proportions would be in keeping with that of the main property and the proposed loft conversion and rear dormer are therefore considered acceptable. The rooflights on the front roof slope are considered visually acceptable in the street scene.
- 5.5 To conclude, the proposed development would satisfactorily integrate within the character of the existing property and would not be harmful to the Crabbet Road street scene. It would therefore be in accordance with the Local Plan Policies, the design guidance in the SPG 5 and the relevant paragraphs of the NPPF.

The impact on neighbouring properties and amenities

- 5.6 The properties most affected by the proposed extensions and loft conversion are No. 38 and No.42 Crabbet Road and No.139 The Birches.
- 5.7 Due to the modest nature of the proposed south-east facing front extension as amended and the separation distance from the neighbouring properties No. 38 and No.42 Crabbet Road, there is not considered to be any impact in terms of overlooking, loss of privacy or light.
- 5.8 The proposed single storey rear extension would project approximately 3.3 metres and the adjoining property to the west No.38 Crabbet has a rear single storey conservatory with a solid wall adjacent to the shared boundary and the rear garden boundary is also marked by 1.8m high fencing. Therefore, it is considered that the proposed single storey rear extension would not cause any adverse impact on the amenities enjoyed by the occupants of this neighbouring property.
- 5.9 Given the separation distance with No.42 Crabbet Road and that the proposed rear extension would follow the existing side building line and would be within the applicant’s boundary, it is not considered that there would be any detrimental impact on the amenities enjoyed by the occupants of this neighbouring property nor there would be a restricted access to the neighbour’s rear garage despite the expressed concerns and the new fence.
- 5.10 Regarding the loft conversion, the dormer would introduce 2 windows in the rear elevation which would overlook the property No.139 The Birches to the north. Given the retained gap of approximately 30 metres between the proposed loft conversion and the rear elevation of this neighbouring property, it is not considered that these additional windows would result in a harmful level of overlooking and loss of privacy to this property despite the stated concerns. There are not considered to be any issues with overlooking from the front rooflights.
- 5.11 To conclude, the proposed single storey front and rear extensions and loft conversion would not have any detrimental impact on the amenities enjoyed by the occupants of neighbouring properties and would accord with the Local Plan Policies, the SPG 5 and the NPPF.

Parking considerations

- 5.12 The proposal would create an additional bedroom resulting in the property becoming a 4 bedroom dwelling. During the course of the determination of the application as amended parking plan has been received showing to the front of the dwelling 2 parking spaces. As a result, there would be sufficient parking provided and it is considered that a condition is required to ensure that adequate and satisfactory provision is made for the parking of vehicles clear of the highway to allow the enlarged dwelling to meet its operational requirements in accordance with policy CH3 of the Crawley Borough Local Plan 2015-2030 and the adopted parking standards which require a maximum of 2-3 spaces.

CONCLUSIONS:-

- 6.1 In conclusion, it is considered that the scale and design of the proposal is acceptable, would not have an adverse impact on the character and appearance of the dwelling and the street scene and would not have any detrimental impact on the parking arrangements and the amenities enjoyed by the occupants of neighbouring properties. The proposed single storey front and rear extensions and loft conversion are considered to accord with the policies outlined in the NPPF (2012), the Crawley Borough Local Plan (2015-2030), the Supplementary Planning Guidance Note: 5 – Residential Extensions (2001) and the Supplementary Planning Document 2008 – Planning Obligations and S106 Agreements, and it is recommended to grant permission for this application subject to conditions.

RECOMMENDATION RE: CR/2016/0193/FUL

PERMIT - Subject to the following conditions:-

1. The development hereby permitted shall be begun before the expiration of 3 years from the date of this permission.
REASON: To comply with Section 91 of the Town & Country Planning Act 1990.
2. The development hereby permitted shall not be carried out other than in accordance with the approved plans as listed in this Decision Notice save as varied by the conditions hereafter.
REASON: For the avoidance of doubt and in the interests of proper planning.
3. The materials and finishes of the external walls and roofs of the buildings hereby permitted shall match in colour and texture those of the existing building.
REASON: In the interests of amenity in accordance with Policies CH3 of the Crawley Borough Local Plan 2015-2030.
4. The extensions and loft conversion shall not be occupied unless and until the 2 car parking spaces, as shown on drawing No. 4837-010 Rev A Amended Proposed Site Plan with Parking, have been provided and constructed. These parking spaces shall be retained as parking spaces unless otherwise agreed in writing by the Local Planning Authority.
REASON: To ensure adequate provision of parking clear of the highway in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.

NPPF Statement

In determining this planning application, the Local Planning Authority assessed the proposal against all material considerations and has worked with the applicant in a positive and proactive manner based on seeking solutions where possible and required, by:

- Liaising with applicant and agent and discussing the proposal where considered appropriate and necessary in a timely manner during the course of the determination of the application.
- Seeking amended plans to address identified issues during the course of the application.

This decision has been taken in accordance with the requirement in the National Planning Policy Framework, as set out in article 35, of the Town and Country Planning (Development Management Procedure) Order 2015.



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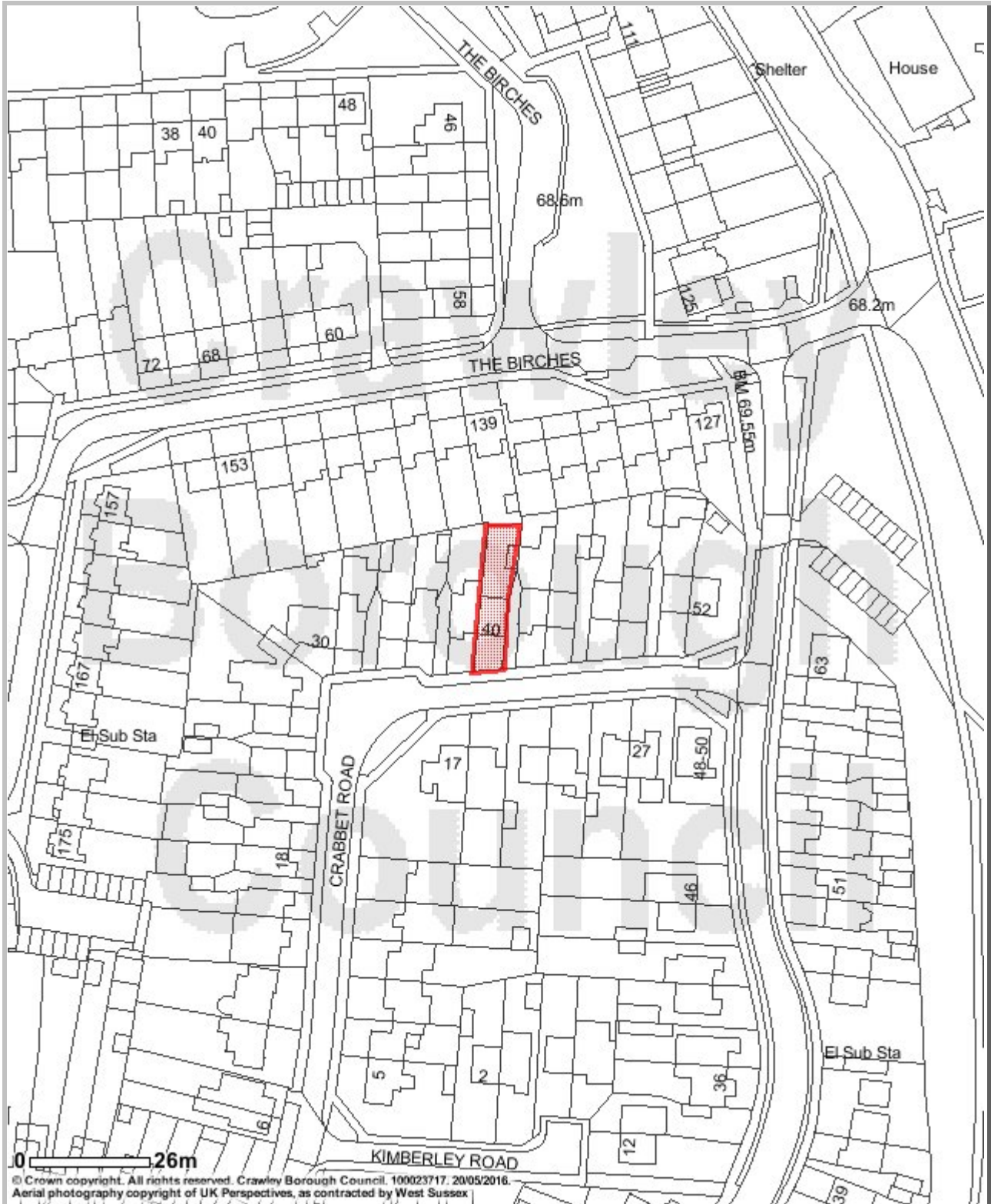
Tel: 01293 438000
Fax: 01293 438603

CR/2016/0193/FUL

Date 17 May 2016

Approx. Scale 1:1,250

**40 CRABBET ROAD, THREE BRIDGES, CRAWLEY,
RH10 1NE**



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REFERENCE NO: CR/2016/0216/FUL

LOCATION: [UNIT 1, POWER AVENUE, \(PART OF FORMER GSK SITE\), NORTHGATE, CRAWLEY](#)
PROPOSAL: ERECTION OF SECONDARY ROOF OVER THE EXISTING ROOF MOUNTED PLANT WITH ASSOCIATED CHANGES TO THE EXISTING ELEVATION SCREENING

TARGET DECISION DATE: 12 August 2016

CASE OFFICER: Mrs J. McPherson

APPLICANTS NAME: Digital Realty Crawley 2 SARL

AGENTS NAME: Nicholas Webb Architects

PLANS & DRAWINGS CONSIDERED:

0378-366 Rev P1 Proposed GA Elevations, 0378-368 Rev P1 3D Section A-A

CONSULTEE NOTIFICATIONS & RESPONSES:-

- | | | |
|----|--------------------------------------|---|
| 1. | GAL - Aerodrome Safeguarding | No objection subject to condition and informative |
| 2. | National Air Traffic Services (NATS) | No objection |
| 3. | CBC - Drainage Officer | No objection |
| 4. | CBC - Environmental Health | No comments received |
| 5. | WSCC Highways | Reply awaited |

NEIGHBOUR NOTIFICATIONS:-

The application was publicised by press notice and site notices.

RESPONSES RECEIVED:-

None.

REASON FOR REPORTING TO COMMITTEE:-

The application is 'major' development.

THE APPLICATION SITE:-

- 1.1 The application relates to the recently erected Digital Realty Data Centre building (Use Class B8) situated on the south eastern part of the former GSK site on the southern boundary of the Manor Royal Business District. The building is situated on the eastern side of the new spine road which now intersects the site (known as Power Avenue).
- 1.2 To the south is the Northgate residential neighbourhood, separated from Manor Royal by the A2011 dual carriageway. To the north and east are business uses with Magpie Wood located further to the west. The whole of the GSK site was the subject of an outline planning permission for a mixed-use employment park granted originally in 2012.
- 1.3 There is a narrow belt of trees along the frontage to Crawley Avenue (to south) and associated landscaping and trees have been planted on the frontage of the building along Power Avenue.

THE PROPOSED DEVELOPMENT:-

- 2.1 The application seeks planning permission for the erection of a new secondary roof above the generator compound and plant which is currently all mounted on the roof of the building and is concealed from wider views by a parapet wall. The existing cladding at the top of the building would be extended upwards and in places replaced with louvres, the new roof would be set behind the cladding and comprise a shallow profile 'm' shape with new roof vents for the plant being created above the parapet.
- 2.2 The oversailing roof would be installed to provide additional protection to the installed roof plant and to enable ease of maintenance regardless of weather conditions.

PLANNING HISTORY:-

- 3.1 Outline permission was originally granted for the development of the former GSK site for a mixed employment park under CR/2012/0134/OUT. This included B1(c) (light industrial), B2 (general industrial), B8 (storage and distribution) and a Business Hub accommodating a mix of uses including B1a (Offices), B1c (light industrial), B8 (storage and distribution), C1 (hotel), A1 retail, A3 (restaurants and cafes), A5 (hot food take way) and a car dealership/s which is sui-generis. This application sought approval for the vehicular access arrangements for the development only. Landscaping, layout and scale were reserved for future consideration. An application for the approval of the design for the spine road, linking Crawley Avenue to Manor Royal was then approved under CR/2012/0134/ARM.
- 3.2 A subsequent application was then submitted for the southern section of the site fronting Crawley Avenue together with the a site that was formerly occupied as a manufacturing and research/development site owned by Carpenter Technology (UK) Ltd under reference CR/2013/0255/FUL for a new data storage facility (Use Class B8). The facility was to be provided in 2 main single storey buildings one of which has now been erected and is the subject of this application. The remainder of the site currently remains undeveloped.

PLANNING POLICY:-

- 4.1. The National Planning Policy Framework 2012 (NPPF) has a golden thread running through it which seeks to ensure a presumption in favour of sustainable development and promote good design. The Framework requires planning applications to be determined in accordance with the development plan.

Crawley 2030: The Crawley Borough Local Plan 2015-2030

- 4.2 Policy SD1: (Presumption in favour of Sustainable Development)
- 4.3 Policy CH2: (Principles of Good Urban Design) States that all proposals for development in Crawley will be required to respond and reinforce local distinctive patterns of development and landscape character, and create continuous frontages onto streets and spaces enclosed by development which clearly defines private and public areas.
- 4.4 Policy CH3: (Normal Requirements of All New Development) states all proposals for development in Crawley will be required to make a positive contribution to the area; be of a high quality design, provide and retain a good standard of amenity for all nearby and future occupants of land and buildings and be able to meet its own operational requirements necessary for the safe and proper use of the site.
- 4.5 Policy EC3: states that Manor Royal is the principal business location for Crawley, and instrumental to the success of the wider Gatwick Diamond. Development that is compatible with the area's economic function and role in the wider sub-region will be permitted where it falls within the B Use Class and would result in the reuse, intensification, or change of use of the land or buildings. All development should contribute positively to the overall setting and environment of the Main Employment Area.

- 4.6 Policy ENV6: (Sustainable Design and Construction) requires all development to achieve BREEAM excellent for water and energy credits where viable.
- 4.7 Policy ENV7 : District Energy Networks identifies the site with a priority area where major development should consider developing its own system, consider the use of site-wide communal energy systems or be 'network ready' unless the objectives cannot be achieved due to technical or financial viability or due to site or development constraints.
- 4.8 Policy ENV9: tackling Water Stress: seeks non-residential development to achieve BREEAM excellent for water efficiency where technically feasible.
- 4.9 Policy IN1 Infrastructure Provision – development will be permitted where it is supported by the necessary infrastructure both on and off site and CIL or S106 agreements where appropriate will address site specific issues.
- 4.10 Policy IN4 (Car and Cycle Parking Standards) States that development will be permitted where the proposals provide the appropriate amount of car and cycle parking to meet its needs when it is assessed against the borough Councils car/cycle parking standards.

Supplementary Planning Documents

- 4.11 The Manor Royal Design Guide and the Manor Royal Public Realm. The Design Guide SPD recognises that improvements are required to the public realm through enhancing important private frontages and gateway sites. The SPD identifies where such improvements are needed, having regard to the location of sites, and setting out where the use of design principles, or the provision of structural landscaping, could provide improvement to both the private and public realms. The SPD also provides general design principles for the whole area and aims to deliver improvements to the setting and environment of Manor Royal as a means of enhancing its position in the sub-region and as a local employment destination. It does not seek to prescribe the types of use that will be considered acceptable in Manor Royal.
- 4.12 The Planning Obligations and Section 106 Agreements SPD sets out the parking standards for development and details on infrastructure contributions such as TAD and for Manor Royal Improvements.

PLANNING CONSIDERATIONS:-

- 5.1 The main considerations in the determination of the application relate to:
 - the impact that the proposed new roof would have on the character and appearance of the building and surrounding area,
 - sustainability and environmental impacts;
 - the impact on the operational requirements of the building.
- 5.2 In relation to the design and visual impact, the alterations are to the top of the building extending the existing cladding and in sections replacing the existing cladding with louvres. The new roof of the building would not be visible from street level however, the skyline on the building would appear different with the introduction of air intake vents (spaced regularly over the roof) to provide additional ventilation to the plant, these features would be finished with louvres to match the walls.
- 5.3 The proposed roof follows the design of the original building and would retain the 'book end' concept of the façade which adds visual interest to the building. The proposal is considered to be in scale with the existing building and as such would maintain its modern appearance and not detract from its overall appearance. It is also considered appropriate given the building's siting and context within the main employment area and would not detract from intended design of other buildings in the surrounding area (as yet unimplemented).
- 5.4 Given the building would provide an additional roof over the existing roof plant it is not considered to raise any further impact in terms of noise generated from the building and associated plant. It is

however considered appropriate to reapply the condition regarding Bird Hazard Management as required by Gatwick Safeguarding which was attached to the original permission for the building.

- 5.5 In respect of sustainability measures the building was designed to achieve BREEAM 'very good', the new roof will continue to allow for the reclamation and storage of water which is currently part of the design of the building. Furthermore, the secondary roof will address the technical issues with the plant and energy efficient cooling system that have arisen in the first year of operation and should ensure the benefits of the technology are retained longer term. The building has already been conditioned to be 'network ready'.
- 5.6 In this case there are no impacts on the parking or operational requirements for the building. The design is such that the enclosed area is only for use as plant floor (the sides are not enclosed) and the applicants confirmed this alteration would not result in any increase in employees or activity at the building. There is no requirement for additional parking for staff and therefore the operational requirements of the building are not affected. It is considered that the use of the upper plant floor should be restricted via condition to ensure it is not later converted to office space by this occupant or a future occupier.
- 5.7 In respect to infrastructure contributions (TAD and Manor Royal improvements), these are calculated on the basis of the creation of new floorspace and in the case of TAD are justified based on the impact of the development on the transport network and Manor Royal based on the number of new employees. In this case, while the second roof has in effect created a new floor, the space provided is enclosure of the existing plant only and is not creating any additional office floorspace or capacity for new employees. In these circumstances as the space is clearly only to provide additional protection to the operational plant and is not expanding the employment capacity of the building, additional contributions are not considered appropriate in this instance.

CONCLUSIONS:-

- 6.1 The proposed design alterations to the building are considered visually acceptable, do not alter the operational requirements of the building and should result in improvements to the building's energy efficiency, as a result it is considered that the proposal accords with all the relevant development plan policies and planning permission should be granted.

RECOMMENDATION RE: CR/2016/0216/FUL

To delegate decision to Head of Economic and Environmental Services to await expiry of the consultation period on 15 June 2016 and consideration of any late representations received during this period with view to **PERMIT** - Subject to the following condition(s):-

1. The development hereby permitted shall be begun before the expiration of 3 years from the date of this permission.
REASON: To comply with Section 91 of the Town & Country Planning Act 1990.
2. The development hereby permitted shall not be carried out other than in accordance with the approved plans as listed in this Decision Notice and materials set out within the application save as varied by the conditions hereafter.
REASON: For the avoidance of doubt and in the interests of proper planning.
3. The Bird Management Hazard Management Plan dated February 2014 as required under CR/2013/0255/CC2 shall be implemented as approved upon completion of the roof. No alterations to the approved Bird Hazard Management Plan shall take place unless submitted to and approved in writing by the Local Planning Authority.
Reason: It is necessary to manage the roof in order to minimise its attractiveness to birds which could endanger the safe movement of aircraft and the operation of Gatwick Airport.

4. No development shall be carried out unless and until a schedule of materials and finishes and, where so required by the Local Planning Authority, samples of such materials and finishes to be used for external walls of the building have been submitted to and approved by the Local Planning Authority. REASON: To enable the Local Planning Authority to control the development in detail in the interests of amenity by endeavouring to achieve a building of visual quality in accordance with Policy CH3 of the Crawley Borough Local Plan 2015-2030.
5. The additional floorspace created by the enclosure of the original roof shall be used only for the installation of plant and ancillary equipment associated with the use of the building as a datacentre and shall not be converted, adapted or used for any other purposes. REASON: To ensure that the building can meet its operational requirements in respect of parking provision in accordance with policy IN4 of the Crawley Borough Local Plan 2015-2030.

Informative

- 1 Given the nature of the proposed development it is possible that a crane may be required during its construction. The applicant's attention is drawn to the requirements of the British Standard Code of Practice of the safe use of cranes, for crane operators to consult the aerodrome before erecting a crane in close proximity to an aerodrome. Gatwick Airport requires a minimum of four weeks notice. For crane queries/applications please email gal.safeguarding@gatwickairport.com. The crane process is explained further in Advice Note 4 'Cranes and Other Construction Issues' available at www.aoa.org.uk/policy-campaigns/operations-safety/

1. NPPF Statement

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development, as set out within the National Planning Policy Framework.

This decision has been taken in accordance with the requirement in the National Planning Policy Framework, as set out in article 35, of the Town and Country Planning (Development Management Procedure) Order 2015.



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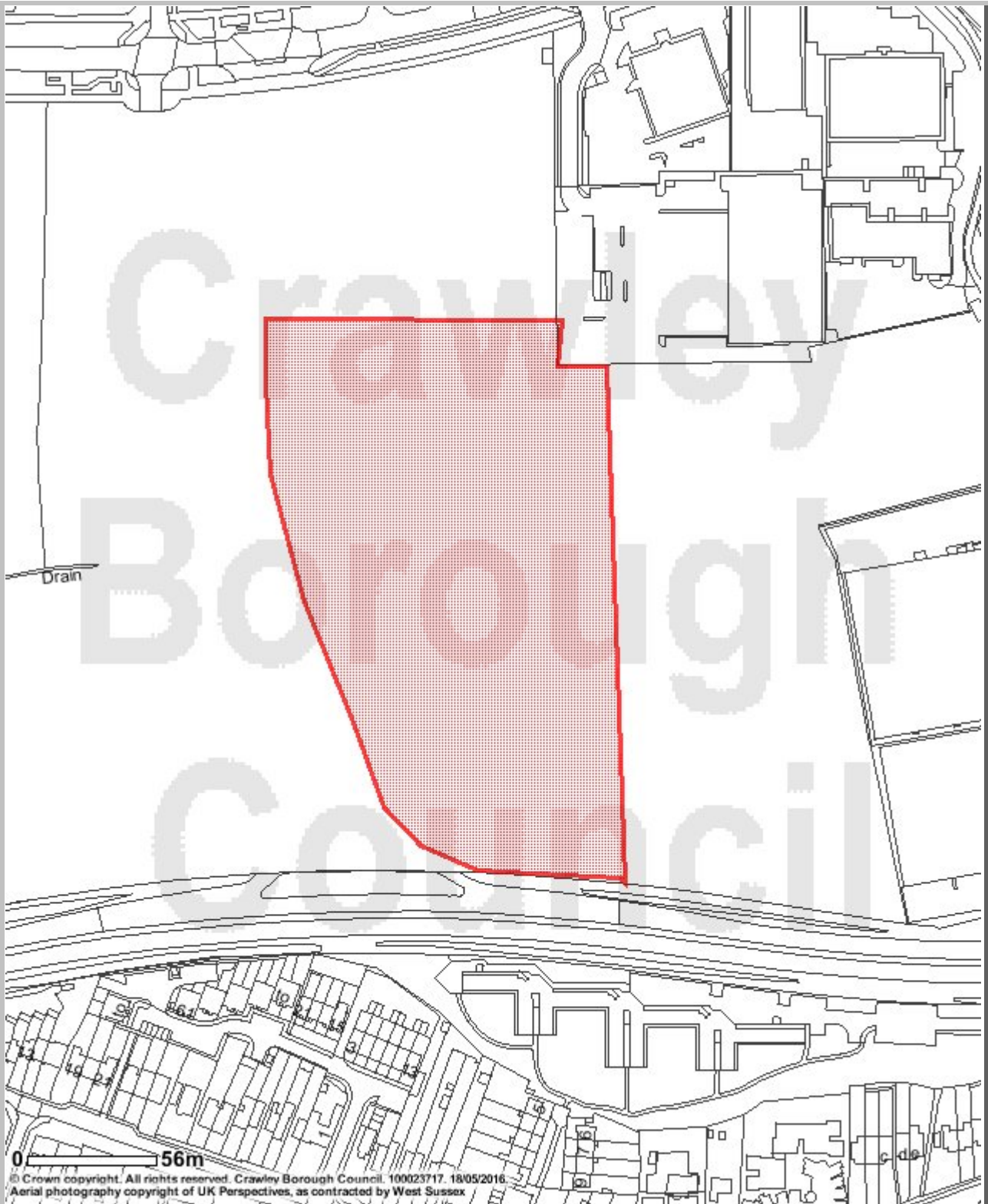
Tel: 01293 438000
Fax: 01293 438603

CR/2016/0216/FUL

Date 18 May 2016

Approx. Scale 1:1,250

**UNIT 1, POWER AVENUE, (PART OF FORMER
GSK SITE), NORTHGATE, CRAWLEY**



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REFERENCE NO: CR/2016/0225/FUL

LOCATION: [27 FORGE ROAD, THREE BRIDGES, CRAWLEY](#)

PROPOSAL: CHANGE OF USE OF AMENITY LAND TO PRIVATE GARDEN. (REVISED LOCATION PLAN AND BLOCK PLAN).

TARGET DECISION DATE: 23 June 2016

CASE OFFICER: Mr C. Peters

APPLICANTS NAME: Miss Y Ghulam

AGENTS NAME:

PLANS & DRAWINGS CONSIDERED:

Front, Side, and Rear Elevations - Existing, Block Plan, Location Plan

CONSULTEE NOTIFICATIONS & RESPONSES:-

- | | |
|--|---|
| 1. Highways: | From an inspection of WSCC maintainable highway records the land in question does not appear to be either maintained or in ownership of WSCC. The change of use of the land should be determined by the owner of the land, which appears to be Crawley Borough Council. |
| 2. CBC Property Division: | No objection raised |
| 3. CBC Planning Arboricultural Officer | No objection |

NEIGHBOUR NOTIFICATIONS:-

1 The Birches, Three Bridges; 18 Forge Road, Three Bridges; 25 Forge Road, Three Bridges; 2 Pond Wood Road, Three Bridges; 1 The Birches Three Bridges; 9 Rye Ash, Three Bridges; 1 Brookside, Three Bridges; 15 Scallows Close Three Bridges; 109 Hazelwick Road, Three Bridges;

RESPONSES RECEIVED:-

Following neighbour notification, 16 letters of representation were received from 13 individual addresses. The main issues raised were as follows:

- The application site is not a family home and the owner does not live at the property.
- The existing property is untidy and uncared for.
- Concerns that this land is wanted in order to build a further extension in the future.
- Potential damage to tree roots

REASON FOR REPORTING TO COMMITTEE:-

This application is reported to committee due to the number of representations received and the recommendation to permit.

THE APPLICATION SITE:-

- 1.1 The application site relates to a two storey-semi-detached dwelling located on the northern side of Forge Road and situated at the junction with The Birches. To the east of the rear boundary of the

site, along the eastern side boundary a row of 4 garages are located, of which the nearest of these garages is under the ownership of the applicant. At the site frontage immediately to the east of the existing driveway, is an area of Council owned grassed amenity land, which is approximately 13ms wide. On this land two large trees are present. The front driveway to the dwelling is presently capable of providing 1 off road parking space, and is surfaced with broken bricks, gravel and paving slabs.

- 1.2 Within the rear garden of the application site is a large Oak tree that is covered by Tree Preservation Order (TPO) number 16.10.57, which was confirmed on 03.08.2012. Other than this protected Oak tree there are no other identified site constraints. The surrounding area is residential in nature and bar is predominantly characterised by rows of terraced dwellings. Immediately to the east of the site is The Birches amenity land.

THE PROPOSED DEVELOPMENT:-

- 2.1 This application seeks planning permission for the change of use of land from Council owned public amenity land to private amenity land to be incorporated into the residential curtilage of No 27 Forge Road and, which would form an enlargement of the applicants existing driveway. The land in question is located immediately to the east of the existing driveway and is grassed which forms the corner of Forge Road, at the junction with The Birches. The piece of land involved is 1.2ms wide, with a length of 8.5ms.

PLANNING HISTORY:-

- 3.1 CR/2015/0472/FUL - Change of use of council owned amenity land and erection of single storey side and rear extensions to house in multiple occupation – Refused
- 3.2 CR/2013/0566/FUL - Erection of single storey side and rear extensions to a house in multiple occupation (Use Class C4) (amended description) – Permitted
- 3.3 CR/2013/0338/192 - Certificate of lawfulness for proposed single storey side extension - Withdrawn

PLANNING POLICY:-

- 4.1 National Planning Policy Framework 2013 (NPPF):

The core planning principles of the NPPF (paragraph 17) states that planning should proactively drive and support sustainable economic development to deliver the homes, business and industrial units, infrastructure and thriving local places that the country needs. Every effort should be made objectively to identify and then meet the housing, business and other development needs of an area, and respond positively to wider opportunities for growth.

In addition development should secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings.

- Paragraph 56 attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.

- 4.2 Crawley 2030: The Crawley Borough Local Plan 2015-2030

- Policy SD1: (Presumption in favour of Sustainable Development) In line with the planned approach to Crawley as a new town, and the spatial patterns relating to the neighbourhood principles, when considering development proposals the Council will take a positive approach to approving development which is sustainable.
- Policy CH2: (Principles of Good Urban Design) States that all proposals for development in Crawley will be required to respond and reinforce local distinctive patterns of development and

landscape character, and create continuous frontages onto streets and spaces enclosed by development which clearly defines private and public areas.

- Policy CH3: (Normal Requirements of All New Development) states all proposals for development in Crawley will be required to make a positive contribution to the area; be of a high quality design, provide and retain a good standard of amenity for all nearby and future occupants of land and buildings and be able to meet its own operational requirements necessary for the safe and proper use of the site.
- Policy IN7 (Crossovers): States that proposals for crossovers to access hardstandings in front gardens will only be permitted where highway safety and the amenity of the street scene are not adversely affected; and the proposal will not result in the unacceptable loss of public on-street parking spaces.

PLANNING CONSIDERATIONS:-

5.1 The main considerations in the determination of this application are:

- Design & appearance of the proposal & visual impact on the street scene
- Impact on trees
- Other matters

Design & appearance of the proposal & visual impact on the street scene

5.2 The proposed development would result in the change of use of a 1.2m wide strip of land which immediately adjoins the driveway of no.27 Forge Road. At present the strip of land is grassed, however the applicant has confirmed that they intend to use this parcel of land to form an enlargement of their existing driveway. Following the implementation of this development, the retaining amenity land would maintain a width of 11.1ms which would be a substantial proportion of the land available. Therefore, the loss of 1.2ms of public amenity land would not result in an unacceptable loss of visual public amenity and would not be detrimental to the character of the area. The proposed development is considered acceptable in this regard and would accord with Local Plan 2015- 2030 Policies CH2, CH3 and IN7.

Impact on trees

5.3 The applicant has advised that the resultant driveway extension would be constructed of brick paving. Having consulted the Councils Tree Officer on the acceptability of the development, no objection has been raised subject to a condition ensuring that a 'no-dig' construction method is adopted for the driveway. On this basis the proposed change of use of this parcel of land would not have an adverse impact upon the retained trees on the adjacent land and is therefore considered acceptable in this regard.

5.4 Under the most recently considered application at this site (CR/2015/0472/FUL) one of the reasons for refusal related to ground works associated with the extension which would have disturbed the roots of the retaining trees. As the proposed development would not involve the building of structures or digging of any foundations, and for the reasons as outlined above, no such concerns are raised in this instance.

Other considerations

5.5 It is noted that the applicant does not live at the application site, however, the owner of a site is not required to reside there in order to apply for permission. The concerns relating to the site being untidy have been noted, and should the Council consider it appropriate to do so they may serve on the owner and occupier a Notice under Section 215 of the Town and Country Planning Act, 1990. If the applicant wished to build any additional extensions, then this would be considered under a separate planning application. Subject to the applicant being able to obtain planning permission for the change of use of this land, the Council as land owner has agreed to sell the land to the applicant.

- 5.6 All of the comments raised within the letters of representations and consultee responses have been fully taken into consideration in forming a recommendation of this application. Having read all of the representations it is concluded that in the absence of harm, there is no reason in planning terms for the change of use of this small piece of land to not be deemed acceptable.

CONCLUSIONS:-

- 5.7 For the reasons as outlined above, the proposed change of use of public amenity land to residential curtilage of no.27 Forge Road would not have an adverse impact upon the character of the streetscene trees, nor would it have an adverse impact upon the amenities enjoyed by the occupants of neighbouring properties. Therefore, on this basis the proposed development is considered to comply with Local Plan 2015-2030 Policies SD1, CH2, CH3 and IN7, and is thus considered acceptable.

RECOMMENDATION RE: CR/2016/0225/FUL

1. The development hereby permitted shall be begun before the expiration of 3 years from the date of this permission.
REASON: To comply with Section 91 of the Town & Country Planning Act 1990.
2. The development hereby permitted shall not be carried out other than in accordance with the approved plans as listed in this Decision Notice save as varied by the conditions hereafter.
REASON: For the avoidance of doubt and in the interests of proper planning.
3. The hardstanding and crossover shall not be used until provision has been made within the site in accordance with details to be submitted to and approved by the Local Planning Authority to prevent surface water discharging onto the public highway.
REASON: In the interests of road safety in accordance with policy CH3 of the Crawley Borough Local Plan 2015-2030.
4. The exact details of the of the driveway finish including method statement of the new driveway shall be submitted to the Council prior to commencement of development. The driveway shall be of 'no-dig' construction with a permeable surface in accordance with British Standard 5837: 2012 Trees in relation to design, demolition, and construction.
REASON: To avoid undue damage to the rooting zone of the adjacent Oak trees in accordance with policy CH2 and CH3 of the Crawley Borough Local Plan 2015-2030.

INFORMATIVE

1. The applicant is advised that the granting of planning permission is separate from any Landlord consent that may be required prior to the implementation of the works.

NPPF Statement

1. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development, as set out within the National Planning Policy Framework.

This decision has been taken in accordance with the requirement in the National Planning Policy Framework, as set out in article 35, of the Town and Country Planning (Development Management Procedure) Order 2015.



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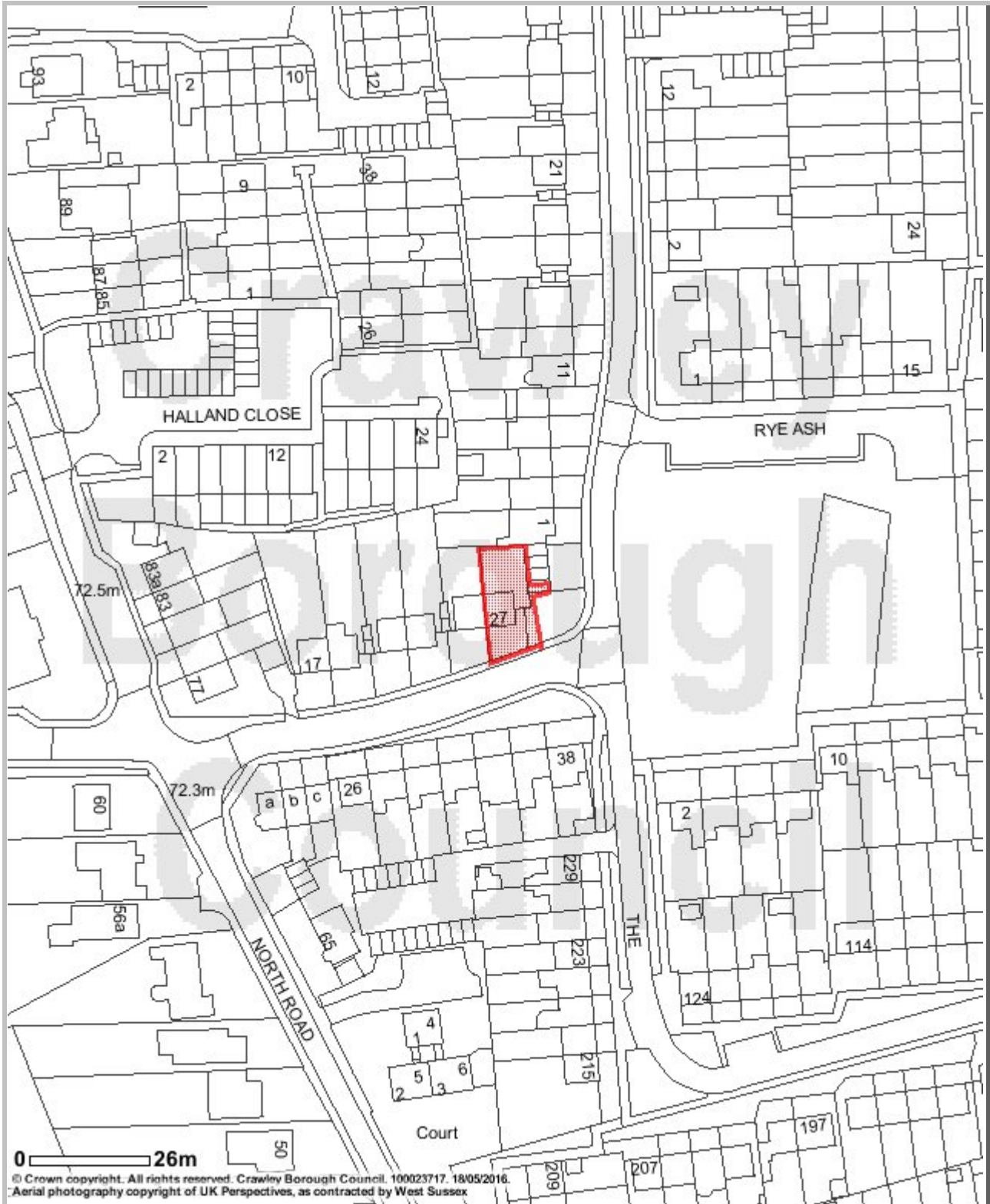
Tel: 01293 438000
Fax: 01293 438603

CR/2016/0225/FUL

Date 17 May 2016

Approx. Scale 1:1,250

27 FORGE ROAD, THREE BRIDGES, CRAWLEY



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REFERENCE NO: CR/2016/0262/FUL

LOCATION: [ROUNDAABOUT ADJ TO MAIDENBOWER DRIVE, MAIDENBOWER, CRAWLEY](#)
PROPOSAL: RELOCATION OF ARTWORK WITH THREE CIRCULAR HOLLOW STEEL METAL
POLES 7M IN LENGTH & ROTATING FIGURES FIXED TO THE TOP FROM WORTH
WAY TO MAIDENBOWER DRIVE, ADJACENT TO THE ROUNDAABOUT

TARGET DECISION DATE: 20 June 2016

CASE OFFICER: Mr A Taylor

APPLICANTS NAME: Crawley Borough Council
AGENTS NAME:

PLANS & DRAWINGS CONSIDERED:

WIE SA 20 001 A02 Details of Art Work, WIE SA 20 002 A01 Location Plan, WITH FIGURE WIE-13552-142-SA-20-001-0001

CONSULTEE NOTIFICATIONS & RESPONSES:-

NEIGHBOUR NOTIFICATIONS:-

1 to 11 Stanford Court, Maidenbower; Crawley Borough Council Maidenbower Community Centre, Harvest Road; WSCC Maidenbower Children and Family Centre, Harvest Road.

RESPONSES RECEIVED:-

No responses received

REASON FOR REPORTING TO COMMITTEE:-

The application has been submitted by the Built Environment department of Crawley Borough Council.

THE APPLICATION SITE:-

1.1 The application site is a large grassed area of land to the south of the Community and Day Care Centre on the corner of Maidenbower Drive and Harvest Road, south-west of the roundabout. The area of land subject to this planning application is owned by Crawley Borough Council. The gradient of the site rises towards the centre of the site. There is a knee rail fencing on the site border with a narrow pavement running along Maidenbower Drive. The site is highly visible in the locality and there are numerous long-distance viewpoints.

THE PROPOSED DEVELOPMENT:-

2.1 This application seeks planning permission for the relocation of artwork with three circular hollow steel metal poles 7m in length & rotating figures from their former site at the footpath entry from the Balcombe Road onto the Worth Way. The applicant has submitted plans showing the proposed location adjacent to Maidenbower Drive and constructional details of the artwork and a montage showing the displayed artwork from the opposite site of Maidenbower Drive.

PLANNING HISTORY:-

3.1 No relevant planning history found

PLANNING POLICY:-

National Planning Policy Framework (2012):

4.1 Chapter 7 (Requiring good design). The Government attaches great importance to the design of the built environment. Good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people.

Crawley Borough Local Plan (2015-2030)

4.2 Policy CH3 (Normal Requirements of All Development) states all proposals for development in Crawley will be required to be of high quality in terms of their urban, landscape architectural design and relate sympathetically to their surrounding in terms of scale, density, height, massing, orientation, views, landscape, layout, details and materials.

Supplementary Planning Guidance and Documents

4.3 SPG11 provides guidance on Public Art and encourages its use in transforming a previously anonymous space and thereby reinforcing identity. SPG11 lists various requirements which public art should seek to meet which are as follows: Imaginative and distinctive; Thought provoking and stimulating; Harmonious and relevant to place; Engaging, absorbing, attractive and distinguishing; Inspiring, intriguing and reflective; Vibrant; and Fun.

PLANNING CONSIDERATIONS:-

The main planning considerations in the determination of this application are:

- Design & Appearance of the proposal
- Highway safety

Design & Appearance of the proposal

5.1 The proposed development is considered to be a prominent addition to the street scene. The artwork is currently being displayed on the land adjacent to Worth Way and was previously accepted in principle and considered to be in accordance with SPG11. The new location will see the three 7m high artworks repositioned on the application site as shown on the submitted visualisation.

5.2 The application site is currently used as amenity land under council ownership and provides landscaping, laid grass and bulbs. The installation of the artworks will only take up a small area of the site and the remaining site area would continue to be used as public amenity land. There have been no objections to the proposal in principle or concerns raised by nearby occupiers.

5.3 In conclusion, it is considered that the proposal conforms with planning guidance outlined in SPG11 and will be of high quality, improving the landscape and street scene thereby conforming with policy CH3 of the CBC Local Plan (2015-2030).

Highway safety

5.4 There have been no objections from WSCC Highways department regarding the proposed repositioning of the artwork to the location on Maidenbower Drive. Therefore the proposal is considered to be in accordance with policy CH3 of the CBC Local Plan (2015-2030) and would not cause any adverse impact on highway safety.

CONCLUSIONS:-

- 6.1 In conclusion it is considered that the design and appearance of the proposal is acceptable and would not have a harmful impact on the character of the area or the wider street scene. Therefore the proposal is in accordance with the policies outlined in the Crawley Local Plan 2015-2030 and SPG11 and it is recommended to grant permission for this application subject to conditions.

RECOMMENDATION RE: CR/2016/0262/FUL

PERMIT subject to conditions.

1. The development hereby permitted shall be begun before the expiration of 3 years from the date of this permission.
REASON: To comply with Section 91 of the Town & Country Planning Act 1990.
2. The development hereby permitted shall not be carried out other than in accordance with the approved plans as listed in this Decision Notice save as varied by the conditions hereafter.
REASON: For the avoidance of doubt and in the interests of proper planning.

NPPF Statement

1. In determining this planning application, the Local Planning Authority assessed the proposal against all material considerations and has worked with the applicant in a positive and proactive manner based on seeking solutions where possible and required, by:
 - Providing advice through pre-application discussions/correspondence.
 - Liaising with consultees/respondents/applicant/agent and discussing the proposal where considered appropriate during the course of the determination of the application.
 - Seeking amended plans/additional information to address identified issues during the course of the application.

This decision has been taken in accordance with the requirement in the National Planning Policy Framework, as set out in article 35, of the Town and Country Planning (Development Management Procedure) Order 2015.



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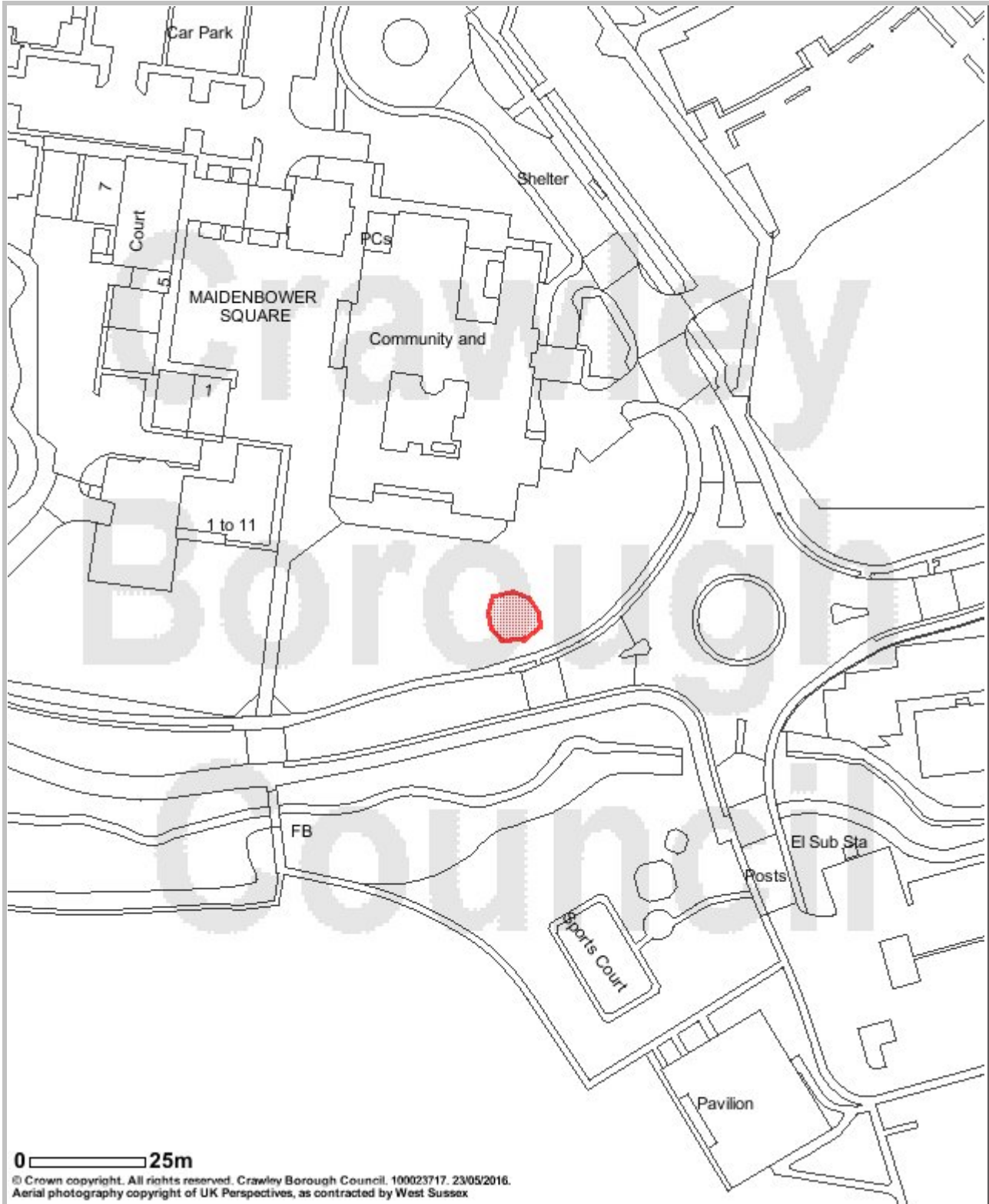
Tel: 01293 438000
Fax: 01293 438603

CR/2016/0262/FUL

Date 3 May 2016

Approx. Scale 1:1,250

**ROUNDBOUT ADJ TO MAIDENBOWER DRIVE,
MAIDENBOWER, CRAWLEY**



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REFERENCE NO: CR/2016/0349/FUL

LOCATION: [UNIT D, WOOLBOROUGH LANE, NORTHGATE, CRAWLEY](#)
PROPOSAL: CHANGE OF USE FROM INDUSTRIAL UNIT WITH ANCILLARY OFFICES TO B8 WAREHOUSE WITH ANCILLARY OFFICES

TARGET DECISION DATE: 27 July 2016

CASE OFFICER: Mr M Edwards

APPLICANTS NAME: Cecil MacDonald and Co Ltd

AGENTS NAME: Vail Williams LLP

PLANS & DRAWINGS CONSIDERED:

CBC 0001 Site Location Plan, TS15-198G1E Block Plan, TS15-198G3 Rev A Proposed Ground Floor Plan, TS15-198G4 Rev A Proposed First Floor Plan

CONSULTEE NOTIFICATIONS & RESPONSES:-

1.	Environment Agency	No response received
2.	WSSC - Highways	No Objection
3.	UK Power Networks	No Objection
4.	CBC - Contaminated Land	No response
5.	CBC - FP - Retail & Employment	No Objection
6.	CBC - FP - Manor Royal	No Objection
7.	CBC - Env Health (AQMA)	No response

NEIGHBOUR NOTIFICATIONS:-

The application was advertised by a press notice and two site notices.

RESPONSES RECEIVED:-

None

REASON FOR REPORTING TO COMMITTEE:-

Major application

THE APPLICATION SITE:-

- 1.1 The application site is located within the Manor Royal Business District at the southwestern most extent of Woolborough Lane, just to the north of Crawley Avenue (A2011). The site comprises a "two storey" (approx. 8m high) rectangular Industrial Unit (approx. 3439 sq.m) with ancillary offices, with service yard to the front with parking along the southern flank.
- 1.2 There is substantial tree screening to the south along the boundary with Crawley Avenue.
- 1.3 Neighbouring uses comprise a mix of industrial, light industrial and storage and warehousing units. The buildings are industrial in appearance and generally 2 storey in scale.

THE PROPOSED DEVELOPMENT:-

- 2.1 The proposal is for a change of use from an “Industrial use and ancillary offices” to a Warehouse Use with ancillary offices (Class B8). The building has been occupied by “Marriot In-Flight Services”. According to the applicant the last occupier used the building for warehousing, although it is not clear whether the use had become lawful by reason of time. The unit is currently vacant and the potential new occupier wishes to occupy the building with the benefit of planning permission, hence the submission of this application.
- 2.2 The proposal would result in no external changes to the building nor any change to the existing access to the highway, parking area or on site manoeuvring facilities.

PLANNING HISTORY:-

3.1 Relevant planning history is as follows:

- CR/406/81- Erection of Industrial Unit and Ancillary Offices- Permitted
- CR/2015/0632/FUL- Alterations to main (east) facade, insertion of five roller shutter doors, two ground floor windows and metal sheet cladding. Removal of existing canopy and provision of glass canopy over office entrance. Re-grading of lorry entrance to allow direct access to proposed roller shutter doors. Reconfiguration of internal space to facilitate installation of roller shutter doors- Approved November 2015
- CR/2015/0632/NM1- Non material amendment of approved application CR/2015/0632/FUL to increase roof height by 190mm, replace cladding, alter shutter door locations and configurations, remove windows/frames and vents, alter internal layout and confirmation of ramp details.- Approved May 2016

PLANNING POLICY:-

4.1 The National Planning Policy Framework 2012 (NPPF) has a golden thread running through it which seeks to ensure a presumption in favour of sustainable development. The NPPF states that there are three dimensions to sustainable development and the planning system performs an economic, social and environmental role. These roles are mutually dependent. The Framework requires applications to be determined in accordance with the development plan.

4.2 Relevant sections are:

- paragraph 14: presumption in favour of sustainable development – this means that development that accords with the development plan should be approved without delay, or where the development plan is absent, silent or relevant policies are out of date, unless there would be significant adverse impacts which would outweigh the benefits or it would be contrary to the policies in the NPPF
- paragraph 17 : core planning principles
- section 7: requiring good design - this emphasises the importance of good design to achieve high quality and inclusive design for all development. Including individual buildings, public and private spaces and wider development schemes. It is proper to seek to promote or reinforce local distinctiveness and the policies and decisions should address the integration of new development into the natural, built and historic environment.

4.3 Crawley 2030: The Crawley Borough Local Plan 2015-2030

Relevant policies include:

- Policy SD1. (Presumption in favour of Sustainable Development) In line with the planned approach to Crawley as a new town, and the spatial patterns relating to the neighbourhood principles, when considering development proposals the Council will take a positive approach to approving development which is sustainable.

- Policy CH2 sets out the ‘Principles of Good Urban Design’ which requires development to amongst other things, create continuous frontages onto streets and spaces with clearly defined private and public areas, respond to and reinforce locally distinctive patterns of development, provide recognisable routes, intersections and landmarks to help people find their way around and provide diversity and choice through a mix of compatible developments and uses that work together to create viable places that respond to local needs.
- Policy CH3 sets out the ‘Normal Requirements of All New Development’ requiring development to be based on a thorough understanding of its context, be high quality in design and related sympathetically to its surroundings. Development should also provide a good standard of amenity for future occupants, demonstrate how secure by design has been included into the design, ensure the development needs its operational needs such as for access, parking, refuse storage and demonstrate building for life 12 Criteria taken into account.
- Policy EC1 (Sustainable Economic Growth) states that Crawley’s role as the key economic driver for the Gatwick Diamond will be protected and enhanced.
- Policy EC2 (Economic Growth in Main Employment Areas) seeks to support proposals for employment generating development in the Main Employment Areas where they contribute to the specific characteristics of the main employment area, and overall economic function of the town, through providing a mix of employment generating uses.
- Policy EC3 (Manor Royal) places a clear focus on the delivery of B-Class business uses within Manor Royal, though outlines that proposals for non B-Class development will be permitted provided that they are of a scale and function that enhances the established role and business function of Manor Royal.
- Policy EN8 (Development and Flood Risk) advises that development proposals must avoid areas which are exposed to an unacceptable risk from flooding, and must not increase the risk of flooding elsewhere.
- Policy ENV10 (Pollution Management and Land Contamination) seeks to prevent new and existing development from contributing to or being put at unacceptable risk from, or being adversely affected by, unacceptable levels of soil, air, water or noise pollution, or land instability.
- Policy IN3 requires development to be concentrated in locations where sustainable patterns of travel can be achieved and development does not have an unacceptable increase in traffic congestion or highway safety.
- Policy IN4 requires development to provide the appropriate amount of car and cycle parking to meet its needs. Car parking standards for commercial development will be based on the particular usage of the premises which will take account of the intensity and requirements of each employment usage and the accessibility of the area by public transport and other sustainable modes.

4.4 Supplementary Planning Guidance and Documents

- Manor Royal Design Guide - (Complemented by Manor Royal Public Realm Strategy) –adopted in July 2013. The Design Guide SPD recognises that improvements are required to the public realm through enhancing important private frontages and gateway sites. The SPD identifies where such improvements are needed, having regard to the location of sites, and setting out where the use of design principles, or the provision of structural landscaping, could provide improvement to both the private and public realms. The SPD also provides general design principles for the whole area and aims to deliver improvements to the setting and environment of Manor Royal as a means of enhancing its position in the sub-region and as a local employment destination.

PLANNING CONSIDERATIONS:-

- 5.1 The existing building was previously used for industrial purposes, and the principle of the proposed B8 use would be in accordance with Local Plan Policies EC2 and EC3 of the adopted Crawley Borough Local Plan (2015-2030). Therefore, the proposal is consistent with relevant economic policies and there is no policy objection to the proposal.
- 5.2 Therefore, the main considerations in the determination of this application are;
- Design & appearance of the proposal & visual impact on the street scene
 - Impact upon the amenities of the neighbouring businesses
 - Impact on the users of the highway & parking provision/operational requirements

Design & appearance of the proposal & visual impact on the street scene

- 5.3 The proposal is for a change of use and there are no proposed changes to the external appearance of the building as part of this application.
- 5.4 Two recent planning applications (see planning history) have been approved for the refurbishment and modernisation of the unit and these works have been carried out. The change of use would have no impact on the visual amenity or character of the street scene.

Impact upon the amenities of the neighbouring businesses

- 5.5 The neighbouring buildings are existing commercial uses and the proposal would not adversely impact upon the occupiers of these buildings.

Impact on the users of the highway & parking provision

- 5.6 There are no proposed changes to the parking or vehicle turning arrangements.
- 5.7 However, the parking standards require that for a B8 use 1 car parking space per 100sq.m must be provided, and 1 lorry parking space per 500 sq.m. The floor space of the building (3439sq.m) would be unchanged and a building of this size would normally require 35 car and 7 lorry parking spaces.
- 5.8 The existing car parking area to the south of the building caters for 46 vehicles and there is sufficient space at the front (east) of the building to cater for the 7 lorry parking spaces. WSCC – Highways were consulted and have raised no highway safety or capacity objections to the proposal. Therefore the proposal is considered to be acceptable in respect of Policy CH3 of Crawley Borough Local Plan 2015-2030.

CONCLUSION

- 6.1 The proposal is considered appropriate within Manor Royal as a B8 warehouse with ancillary offices It is not considered to impact upon neighbouring uses and provides for its operational requirements. It is therefore recommended that planning permission be granted.

RECOMMENDATION RE: CR/2016/0349/FUL

PERMIT - Subject to the following condition(s):-

1. The development hereby permitted shall be begun before the expiration of 3 years from the date of this permission.
REASON: To comply with Section 91 of the Town & Country Planning Act 1990.
2. The development hereby permitted shall not be carried out other than in accordance with the approved plans as listed in this Decision Notice save as varied by the conditions hereafter.
REASON: For the avoidance of doubt and in the interests of proper planning.

NPPF Statement

1. The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development, as set out within the National Planning Policy Framework.

This decision has been taken in accordance with the requirement in the National Planning Policy Framework, as set out in article 35, of the Town and Country Planning (Development Management Procedure) Order 2015.



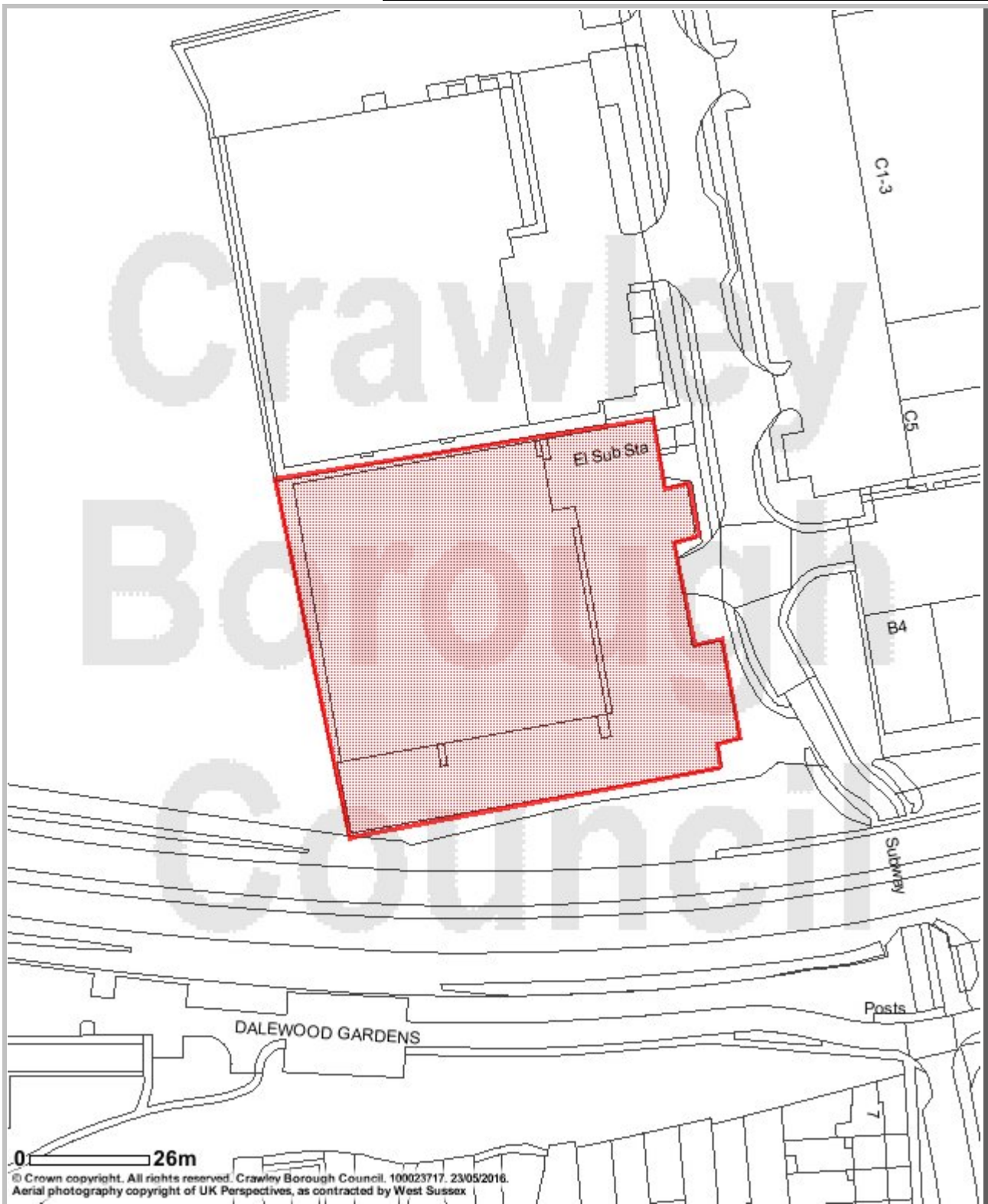
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CR/2016/0349/FUL

Date 19 May 2016

Approx. Scale 1:1,250

**UNIT D, WOOLBOROUGH LANE, NORTHGATE,
CRAWLEY**



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